

**Planning Report**

**Proposed Strategic Housing Development at Lahardane and Ballincolly (Townlands), Ballyvolane, Cork**




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### Appendices

- Appendix A Growing the Opportunities for Cork City's Northside – Cork Chamber Chamberlink Magazine Issue 4 2018 and letters from Cork Chamber of Commerce and Cork Business Association*
- Appendix B Copy of Public Consultation Presentation Boards*
- Appendix C Key Records of Consultation relevant to the Proposed Development*
- Appendix D Part V Calculations*

### Document Control Sheet

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## 1.0 Introduction

### 1.1 Executive Summary

Longview Estates Ltd are seeking planning permission for the development of a residential scheme of 753 no units at Lahardane and Ballincolly, Ballyvolane, Cork. This Planning Report has been prepared by Cunnane Stratton Reynolds and is submitted to An Bord Pleanála to accompany a Section 4 Application for the proposed Strategic Housing Development. The proposed development meets the threshold for Strategic Housing Development (SHD), being a development of 100 or more units on land zoned for residential use, and accordingly is subject to the provisions of the Planning and Development (Housing) and Residential Tenancies Act 2016.

A ten year permission is sought by Longview Estates Ltd for the proposed development of 753 no units, a local centre including a crèche, 2 no. retail units, doctors surgery and community use, public parkland and open spaces, play areas, associated internal roads, pedestrian and cycle paths, boundary treatments, landscaping, lighting, car and bicycle parking, bus stops, bin storage, infrastructure and site services, two no. accesses from the Ballyhooly Road and all associated and ancillary site works.

**Figure 1** Site Location



The proposed development will see the sustainable development of the North Environs of Cork City in an area which has been targeted to accommodate a significant increase in population since the Cork Area Strategic Plan 2001 (and in the 2008 CASP Update). The layout and design being presented to the Board has been through a series of iterations that has seen this current layout being identified as a deliverable and sustainable design solution for this site which is subject to a number of constraints. The proposed scheme has been developed through formal and informal discussions with both Cork County Council and Cork City Council. Pre-

planning discussions have occurred with both councils due to the transfer of the relevant lands from the County administrative area to the City during the design development process. The layout now proposed has also been developed having regard to the feedback from two tripartite meetings with An Bord Pleanála under ABP ref. PL 04.300557 and ABP ref. PL04 .304350. The evolution of the scheme is discussed further below.

This proposal provides for significant public and community infrastructure including a Project Works Services Agreement (PWSA) which has been agreed between Longview Estates Ltd and Irish Water (IW). The proposal provides the foul services infrastructure which will accommodate future residential development that will occur in the Ballyvolane Urban Expansion Area. The proposal includes an Irish Water pumping station which will serve the site, adjoining lands in time and further network extensions, potentially to the northwest.

The proposal through the setbacks incorporated in this scheme accommodates proposed upgrades to the Ballyhooly Road under the Ballyvolane Strategic Transport Corridor Project: North Ring Road to Ballincolly (BSTC). The proposal includes significant upgrades to the Ballyhooly Road that will improve the pedestrian and cyclist environment in this area including a footpath and cycle path from the site down the Ballyhooly Road to connect to the existing footpath network; a bus stop on the Ballyhooly Road and the signalisation of the Ballyhooly Road/Kilbarry Link Road junction.

The delivery of this proposal, in a viable, sustainable manner is important to the overall opening up of this marginalised area of Cork City that has lacked investment in recent years. The opportunity of Cork's northside to contribute to the realisation of the key aims of Project Ireland 2040 for compact and sustainable growth are becoming increasingly recognised. Cork Chamber of Commerce which represents 1,200 members who employ over 100,000 people in the region notes the need for Ireland to become more urban focused and highlights the potential of Cork's northside with its close proximity to the City Centre in "*Growing the Opportunities for Cork City's Northside*"<sup>1</sup>:

*"If we stick with the northside as an example, there is already a hugely impressive portfolio of successful companies with names such as Teamwork.com, Xanadu, Blizzard, Westbourne IT, Strencom, Quintas, Flex and of course Apple. There is the Northside for Business Campus focused on developing enterprises and growing small businesses, and there is a whole host of home grown indigenous businesses. These companies, and others, have recognised the potential here, basing their operations a few short kilometres from the City Centre. This year, we've had positive announcements such as the M20 Cork to Limerick motorway bringing with it the development of the eastern section of the Northern Ring Road. These projects will be massively influential and transformative. Of course, the development of the complete Northern Ring Road route will catalyse this potential even further".*

Cork Chamber is fully supportive of the proposed development at Ballyvolane as stated in the attached letter to the applicant at Appendix A. The Cork Business Association (CBA) also welcomes the proposed development. The letter from CBA (Appendix A) states that Cork is experiencing significant growth in employment space without the necessary provision of residential opportunities for its new and

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<sup>1</sup> *Growing the Opportunities for Cork City's Northside* – Cork Chamber Chamberlink Magazine Issue 4 2018 (Appendix A)

expanding population. CBA further note that the proposed development will encourage investment in the northside and lead the way in encouraging the further focus on residual lands in the Ballyvolane area and Monard further north west of the city.

### 1.2 Design Team

A multidisciplinary and competent design team were involved in the preparation of this planning application including the following:

Longview Estates Ltd - Developer	
Horgan Carroll Architects	
MHL and Associates Ltd – Civil and Transport Engineers	
Cunnane Stratton Reynolds – Town Planners and Landscape Architects	
Arko Visualisation	
Cantwell Keogh Chartered Fire and Safety Consulting Engineers	
John Cronin and Associates – Archaeologists	
Greenleaf Ecologists	
CLV Consulting - Acoustic and Vibration Consultancies	
EC Environmental	

JBA Consulting	
IES Consulting	

### **1.3 Format of the Report**

This Planning Report is structured as follows:

1. Introduction
2. Site Context and Description
3. Description of Proposed Development
4. Evolution of the Proposed Layout/ Design and Pre-Planning Consultations
5. Planning Policy Context
6. Planning History
7. Planning Assessment
8. Conclusion

### **2.0 Site Context and Description**

The proposed development is located in the townlands of Lahardane and Ballincolly, Ballyvolane, on a site of 46.9 hectares in the North Environs of Cork City (Figure 1). The lands are situated less than 3km to the north east of the City Centre and just 1.5km to the north of Ballyvolane District Centre.

The site forms part of the Ballyvolane Urban Expansion Area (UEA) as designated in the Cobh Municipal District Local Area Plan 2017. The 2017 Municipal District Local Area Plans identified nine Urban Expansion Areas throughout Cork County. These are areas that are in principle capable of delivering significant housing output. The planning and implementation strategy to secure the delivery of these sites was identified as a priority for the lifetime of the Local Area Plans. The Ballyvolane expansion area is the closest UEA to Cork City Centre.

The lands the subject of this application (Figure 1) have been farmed over the last number of years. The site is bounded to the north by a local roadway and to the west by the Ballyhooly Road. Presently the lands to the south and east of the site are also in agricultural use. These lands are also zoned as part of the UEA.

There are some 'one off' dwellings located on the local road to the north of the site. There are existing residential estates located in close proximity to the south west of the site at the opposite side of the Ballyhooly Road including Mervue Lawn, Kinvara Close and Brookwood.

**Figure 2** Urban Expansion Areas



There are no significant landscape features on the site which is currently divided up into smaller parcels of land by hedgerows and stone ditches. The site is subject to a number of constraints as follows:

One of the greatest challenges to the development of this site is the steeply sloping topography. The western part of the site closer to the Ballyhooly road is steeply sloping and the site becomes more level as it rises towards the east. This limits opportunities for access from the Ballyhooly Road which would not require significant excavation and achieve compliance with the Design Manual for Urban Roads and Streets (DMURS). There are two areas that are relatively level with the road.

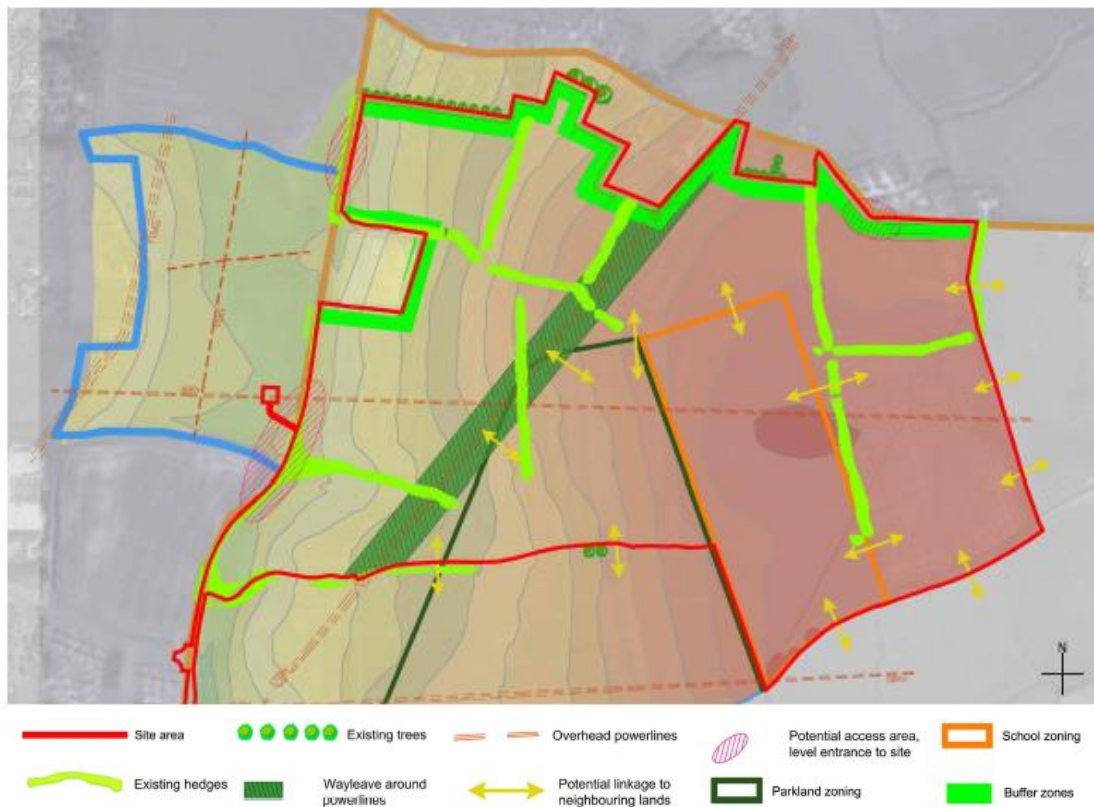
Two overhead power lines traverse the site, namely a significant 110kv line which spans from the south western corner of the site in a north easterly direction and a 38kv overhead power line spanning east-west on the southern portion of the site. While it is proposed to underground the 38Kv power line, the 110Kv is being retained as it is prohibitive economically to underground it. No buildings can be developed under the 110Kv line. This constraint is being used as an opportunity to create a dedicated greenway including the parkland through the site to facilitate connectivity.

The boundary to the north of the site needed to be carefully considered to ensure that there is a sufficient buffer with the existing houses. There is an existing commercial yard to the west of the site that is accessed from the Ballyhooly Road. This site is not within the applicant's ownership and therefore does not form part of this planning application. The sharp level differences between the proposed development and this site requires that the boundaries here are sufficient to ensure safety.

The existing internal hedgerows within the site are a further constraint to its development and therefore have to be removed. The boundary hedgerows are being retained wherever possible. Any loss of hedgerow will be compensated with proposed planting. The landscaping plan for the proposed development results in a net gain in the cover of woodland habitats at the site.



**Figure 3 Site Constraints**



One of the sites main assets is its location. As noted it is less than 3km from Cork City Centre which is the primary employment, retail, commercial and social core in the region. It is also within easy access of the secondary strategic employment locations of Kilbarry and Blackpool which include various employment opportunities at North Point Business Park, City North Business Park, Blackpool Retail Park and Shopping Centre and Kilbarry Business Park. There is a significant amount of established industrial land in Cork City North Environs. The lands accommodate a range of employment generating uses including IT, car servicing and some retail warehousing.

Blackpool Shopping Centre is located approximately 2km to the south west of the site. It is anchored by Dunnes Stores and offers a range of convenience and comparison shopping in smaller stores. Ballyvolane District Centre is located less than 1.5km to the south of the site. Existing facilities in the District Centre include Dunnes Stores and smaller retail units and Lidl. As demonstrated in the EIAR social infrastructure audit there are a number of childcare facilities and schools within the site catchment.

Generally, the North Environs is well served by sporting facilities. There are a number of GAA clubs including Na Piarasigh, Glen Rovers Club, St Vincent's, Delaney's and Mayfield, soccer clubs such as Leeds AFC, Castleview A.F.C, Temple United AFC and Mayfield United. Other leisure and sports facilities include the Glen Sports and Resource Centre, Mayfield Leisure Centre and Riverview Health and Fitness.

In terms of open space provision, Nash's Boreen amenity walk extends from Nash's Boreen up to Hollyhill approximately 2km in length emerging just across from Apple Distribution and Operations (Apple Inc. Campus). The Glen Park and Kempton Park

are also within close proximity of the site. It is noted in the Municipal District LAP that the majority of lands zoned open space are undeveloped or are of no practical use to the public, as they are made up mainly of farmland or vacant grassland with a significant portion of the land steeply sloping.

### **3.0 Description of Proposed Development**

The proposed development will consist of a strategic housing development including 753 residential units to be constructed in a series of phases (six neighbourhoods in total), a local centre including retail (2 no. units), a crèche, doctors surgery and community use unit and all associated and ancillary infrastructure, services and site development works.

The proposed 753 no. residential units are comprised of the following:

- 67 no. detached houses including 31 no. 4 bedroom units and 36 no. 3 bedroom units
- 278 no semi-detached houses including 41 no. 4 bedroom units and 237 no. 3 bedroom units
- 186 no. terrace houses including 18 no. 4 bedroom units, 96 no. 3 bedroom units and 72 no. 2 bedroom units
- 69 no. duplexes including 36 no. 3 bedroom units and 33 no. 2 bedroom units
- 153 no. apartments including 6 no. studio apartments, 42 no. 1 bedroom apartments, 79 no. 2 bedroom apartments and 26 no. 3 bedroom apartments. Three apartment blocks will be provided (2 no. in Neighbourhood 6 and 1 no. in Neighbourhood 2)

The proposed development includes a number of open spaces and play areas in addition to general landscaping, boundary treatments (including walls and landscaping to the houses to the north and lands to the east), and landscaped parkland / greenway. The proposal includes an internal distributor road providing access to neighbouring lands, associated internal roads, car parking, pedestrian and cycle paths (providing access to neighbouring lands), public lighting, internal bus stops and turning area, bin storage (in apartment locations) and cycle parking and all site services infrastructure. The associated site and infrastructural works include water supply, foul and surface / storm water drainage infrastructure to local services and drains and 5 no. unit sub stations. The proposed development makes provision for two no. pumping stations (and connections to / from same), one in neighbourhood 5 and one adjacent to the Ballyhooly Road, with access, to serve this site and future lands as required by Irish Water.

Two no. vehicular accesses are proposed from the Ballyhooly Road and one no. access to / from the local road to the north of the site (pedestrian access points will also be allowed to the local road to the north), all including local road widening within applicant lands, resurfacing and boundary works. Signalisation of the Lower Dublin Hill / Ballyhooly Road Junction is also proposed along with the provision of a new bus stop on the eastern side of the Ballyhooly Road close to the junction of Lower Dublin Hill and the Ballyhooly Road. The application also provides for the reservation of lands to accommodate the widening of the Ballyhooly Road and the provision of new pedestrian and cyclist infrastructure along the eastern side of the Ballyhooly Road with crossing of same close to Mervue Lawn south of the proposed development.

Groundworks, excavation and ground reprofiling are required and proposed to provide a Distributor Road through the site and all development areas internally within the site. The proposed development also provides for the line diversion and partial undergrounding of the Kilbarry-Flaxfort-Mayfield 38kv line that traverses the landholding east / west, the removal of existing pylons and the provision of two new pylons one in the Lahardane Townland and one in the Ballincolly Townland and landscaping works within the 110 kv power line wayleaves that also traverse the site.

The following schedule provides a summary overview of key site and development statistics:

**Table 1** Site Summary Statistics

<b>Total Site Area</b>	46.9Ha
<b>Discountable Site Area</b>	25.73Ha
<b>Developable Site Area</b>	21.09Ha
<b>Number of Units</b>	753 units
<b>Density (Units per Hectare)</b>	35.7 u/ha
<b>Open Space Provision (within neighbourhoods)</b>	15.2%
<b>Open Space Provision (including the greenway)</b>	32%
<b>Car parking provision</b>	1330 spaces
<b>Bicycle space provision</b>	591 spaces

### 3.1 Proposed Phasing

It is intended to develop the proposed development over six phases of development. An indicative phasing plan has been developed by Horgan Carroll Architects and is included with this planning application along with details on the phasing in the Architects Design Report. The final construction sequencing will be confirmed subject to any planning conditions of the decision and once the site contractor has been appointed. A determination on the application is expected from An Bord Pleanála in Q1 2020. Allowing a reasonable period for mobilisation and site set up, potentially works will commence in 2021, subject to the details of a grant of permission and the discharge of any pre commencement planning conditions. The phasing is proposed as follows:

#### *Site Works*

It is intended that initial site works (including access road depending on conditions) will take 6-12 months and include construction of the compound area and storage area (where Neighbourhood 6 is proposed) together with offices and associated welfare facilities and “cut and fill”/ reprofiling of land being developed (including the relocation on site of excess spoil and the storage on site of excess general fill material acceptable for re-use). During this stage it is envisaged that the access roads and internal distributor road are to be constructed (provided for as one full build out or a phased delivery in accordance with housing provision) along with all necessary underground services, ducting etc to accommodate foul and service water demands for later phases. The undergrounding of the 38KV line will be commenced and the construction of all surface water drainage infrastructure to facilitate Phase 1. The areas around the 110KV pylons will be secured for construction safety as agreed with the ESB. Planting and the construction of boundary walls is also proposed along the northern boundary as agreed during consultation with local residents.

Phasing will be broadly consistent with the sequence of Neighbourhoods proposed. However, some of the Neighbourhoods are larger than what can be practically constructed in one year with local construction resources. This will see partial delivery of phases and delivery of envisaged unit numbers as opposed to Neighbourhoods on a year by year basis.

#### *Phase 1 – End 2021*

Phase 1 includes entrance works, the construction of 75 no. units and landscaping for Neighbourhood 1 and roads and services (1-2 units in this portion of the scheme will be permanent show houses until the crèche is open in Phase 2). The construction of retaining structures and embankments to facilitate roads and development areas. This Phase will also see the erection of temporary sales signage at the site entrance and is consistent with Neighbourhood 1.

Phase 1 also allows for local widening of the Ballyhooly Road for the bus corridor and delivery of the permanent cycle lands and pedestrian footpaths for the widened road. Landscaping along the boundary of Phase 6 with the Ballyhooly Road and landscaping along the Ballyhooly Road and at the Irish Water pumping station access is also proposed in this phase. Phase 1 is proposed to include for the new bus stop on the Ballyhooly Road and the pedestrian crossing. Public lighting is also proposed along the Ballyhooly Road in this phase.

Phase 1 also includes the provision of the signalised junction to Lower Dublin Hill/ Ballyhooly Road junction. This phase includes the construction of all underground services, ducting etc to accommodate foul and surface water demands for later phases.

#### *Phase 2 / Phase 3 – End 2022*

Neighbourhood 2 is envisaged as being delivered in two phases from 2022 to end 2023. Phase 2/3 includes the proposed construction of approximately 100 no. units. It includes the Irish Water Type 1 pumping station and associated infrastructure and land take to support the services facilities for the entire Ballyvolane Urban Expansion Area. Ballyhooly Road widening and improvements within the site are proposed in this phase along with internal access distributor road and lighting. This phase includes landscaping to phase 1/2 embankments and the distributor road and the construction of local access to the northern local road. This phase will also include ground works for neighbourhood 2 and park land works as per objective NE-O-04 of the local area plan for the provision of an urban park for public recreation. Phase 2 will commence the provision of the park with the residual lands for the park retained as existing natural hedgerows and features.

Phase 2/3 will include the provision of an internal temporary bus turning bay and the construction of the crèche and local neighbourhood centre. This phase will include the construction of all underground services, ducting etc to accommodate foul and surface water demands for later phases.

#### *Phase 2 / Phase 3 – End 2023*

This phase proposes the construction of approximately 100 no. units and park land provision and public lighting as per objective NE-O-04 of the MD LAP. As noted the balance of the lands zoned for the provision of the urban park in the applicant's control are to be retained as passive amenity. The construction of all underground

services, ducting etc to accommodate foul and surface water demands for later phases are proposed in this phase. Provision is also made for landscaping of this phase.

#### *Phase 4 – End 2024*

Neighbourhood 3 is envisaged as the initial part of Phase 4. Neighbourhood 4 (the portion accessed from the Ballyhooly Road) is envisaged as being delivered as part of this phase also. Phase 4 proposes the construction of 100 no. units, associated landscaping and the second access from the Ballyhooly Road (the access to Neighbourhood 4). This phase includes the construction of the footpath fronting the Ballyhooly Road to the west of Neighbourhood 4 entrance and pedestrian connections / access to the internal park land and roads within the scheme).

#### *Phase 5 – End 2025*

Phase 5 proposes the construction of approximately 100 no. units and associated landscaping in Neighbourhood 5. Phase 5 includes the Irish Water Type 3 pumping station being delivered to support lands to the north east of the site. Phase 5 also includes the construction of all underground services, ducting etc to accommodate foul and surface water demands. . The Balance of housing in Neighbourhood 5 will be delivered in the last Phase of development; Phase 6.

#### *Phase 6*

Phase 6 is proposed to be delivered in two parts as follows:

##### *Phase 6a – End 2026*

This phase proposes 125 no. units; approximately the residual housing in Neighbourhood 5. It is proposed to reduce the size and layout of the construction compound to allow for the construction of the apartments in Neighbourhood 6 at this stage also so as to allow for their construction as a sub stage.

##### *Phase 6b – Mid 2027-2029*

153 no. units are proposed in this phase of the construction. This will include the 126 No apartments in Neighbourhood 6 and the apartments in Neighbourhood 2 if they have not been developed at this stage. The apartments and community space in Neighbourhood 2 may be delivered at an earlier date subject to demand.

All phasing may be subject to Irish Water connection agreements and local connections within phases as required by Irish Water.

Having regard to the scale of the proposed development and the proposed phasing, a ten year permission is sought in this instance under the provisions of Section 41 of the Planning and Development Act 2000 (as amended) to allow sufficient time to construct the proposed development having regard to the local market and construction circumstances.

As per Section 41 of the Act “*without prejudice to the powers conferred on them by this Part to grant a permission to develop land for a limited period only, in deciding to grant a permission under section 34, 37, 37G or 37N, a planning authority or the Board, as may be appropriate, may, having regard to the nature and extent of the*



*relevant development and any other material consideration, specify the period during which the permission is to have effect, being a period —*

*(a) in the case of all development requiring permission, of not less than 2 years, and*

*(b) in the case of residential development requiring permission, of not more than 10 years, and where the planning authority or the Board exercises, or refuses to exercise, the power conferred on it by this section, the exercise or refusal shall be regarded as forming part of the relevant decision of the authority or the Board under section 34, 37, 37G or 37N”.*

The Board in a number of other strategic housing development applications have approved consents for longer than the standard five year period having regard to the scale of the developments proposed and zoning requirements.

It should be noted that the applicant and Cork City Council are proposing to enter into a Section 47 Agreement to regulate the use of the lands in the applicants ownership and to provide certainty regarding the delivery of roads enhancements locally including the delivery of the Mayfield Kilbarry Link Road and the land zoned for a park in the applicants ownership. Under the terms of a Section 47 Agreement, or as otherwise agreed, the Applicant will;

- a) Maintain open access to the “Application Park Area” from completion of the park until such time as the park is taken in charge by the Council.
- b) Maintain the residual c. 17 Ha of lands zoned NE-O-04 in the ownership/control of the Applicant in an open grassland use as specified by the area’s zoning designation. This will continue until the lands are acquired by Cork City Council, should they be required to form part of the 20 Ha Park objective for the Urban Expansion Area, or their use is otherwise defined with the resolution of the alignment of the Mayfield -Kilbarry Link Road to the satisfaction of the Council.

Please refer to Statement of Response to ABP Opinion prepared by Longview Estates Ltd for further detail on the Section 47 agreement.



June 2017 with Mr Sheridan and Mr Paul Killeen of Cork County Council Planning Department and Mr Donald Cronin of the Council's Housing Delivery Unit. The initial sketches were presented to the Council Officers at this meeting.

Following this meeting, the layout and design was progressed and refined and a request for a Section 247 meeting was made on August 25<sup>th</sup> 2017. On the 30<sup>th</sup> August a meeting was held between Mr Peter O'Donoghue (Cork County Council Roads Department), Mr Donald Cronin, Mr Ken Manley of MHL Consulting Engineers and Mr John Crean. In this meeting it was suggested that the applicant evaluate a different design solution for the distributor road which deviated from the adopted alignment in the LAP. The applicants design team committed to undertake the analysis and to consider alternative layouts.

The Section 247 meeting was held on September 1<sup>st</sup> 2017. All relevant staff members of Cork County Council attended and those that could not make it offered feedback by email. Two key items emerged at this meeting – the alignment of the distributor road through the site and housing design.

The layout at this point allowed for a road gradient of less than 8.3%. The Council Housing Delivery Unit and Roads Department suggested a design standard in the region of a 5% gradient, or at least no long sections of road with a slope exceeding 8% and requested that the alignment be altered at its eastern end to take traffic off the local rural road bounding the site to the north.

Another meeting occurred on 11<sup>th</sup> October 2017 between Mr Peter O'Donoghue, Mr Donald Cronin, Mr Ross Palmer (Cork County Housing Delivery Unit), Mr Andrew Hind (Cork County Housing Delivery Unit), Mr Ken Manley (MHL) and Mr David Watson of Longview Estates Ltd. At this meeting, the feedback from the Section 247 meeting and the output of the additional analysis that the design team were asked to explore in the August 2017 meeting were discussed. This meeting led to further modifications being proposed including an increased curvature of the road to provide natural traffic calming as well as sections of slope of 5 degrees at intersections of the estate roads with the distributor road.

In addition to meetings with the Local Authorities, Cork City and County Councillors were invited to a meeting at which the proposed development was presented by members of the design team on 27<sup>th</sup> September 2017. This was to introduce the scheme to Councillors and allow the opportunity for questions to be answered. The following week on October 4<sup>th</sup> 2017 a public consultation event was held for people living in the area of the proposed development. A number of members of the design team were in attendance to talk members of the public through the proposed plans. Please refer to Appendix B for a copy of the presentation material that was on display at the public consultation event.

A further Section 247 meeting was held with Cork County Council on 14<sup>th</sup> December 2017. Refinement of the layout presented at this meeting occurred before the tripartite meeting with the Board in early 2018.

A request for a Section 5 Pre-Application Consultation with An Bord Pleanála was made on 22<sup>nd</sup> December 2017. A tripartite meeting between the design team, An Bord Pleanála and Cork County Council was held at the offices of the County Council on 2<sup>nd</sup> February 2018.

The scheme presented to An Bord Pleanála at this point (ABP ref. PL 04.300557) had 457 no units at a density of circa 18 units per hectare. The design team had

developed a layout for the site on the basis of the adopted Cobh Municipal District Local Area Plan 2017. At this early stage the approach was to develop a layout that was consistent with the LAP as well as other relevant local and national design guidelines.

The feedback from the Board following the tripartite meeting was that amendments were required to constitute a reasonable basis for an application. The following is a summary of the issues highlighted by the Board in the Opinion issued to the prospective applicant in February 2018:

- *Timing and Phasing of Development – further consideration of documents as they relate to the planning rationale for developing at this location having specific regard to the general sequential approach to developing on serviceable lands proximate to urban centres, the topography of the lands and potential environmental and sustainability impact of the proposed development in the wider area, the possible prematurity of development at this location pending the completion of infrastructure related studies and upgrades as set out in the Indicative Development Programme in the Cobh Local Area Plan 2017 and the extent of infrastructure to be delivered in Phase 1 of the proposed development.*
- *Site Topography – further consideration of the documents as they relate to the proposed changes in the topography of the site having specific regard to urban design and environmental considerations given the extent of ‘cut and fill’ activities proposed.*
- *Water and Waste Water Infrastructure – further consideration/ clarification of the documents as they relate to the required extension in water and wastewater infrastructural network to serve the proposed development.*
- *Surface Water Management and Flooding – further consideration of documents as they relate to the potential for increased risk of flooding in the wider area including the Blackpool area.*
- *Traffic and Transportation – further consideration of documents as they relate to the delivery and timing of road infrastructure upgrades in the wider area as identified in the Local Area Plan including connectivity of the proposed development to the urban centre, the proposed urban park and the school site. Further consideration of how the layout, proposed spine road and access arrangements are consistent with the principles of Design Manual for Urban Roads and Streets should be provided.*
- *Residential Density - further consideration of documents as they relate to the residential density of the site. Particular regard should be had to the need to develop at a sufficiently high density to underpin the efficiency of existing or planned public transport services.*
- *Public Open Space – further consideration should be given in relation to the design rationale/ justification outlined in the documents as it relates to the open space proposed particularly in the context of the quantum and usability of the active open spaces on the site and the integration and connections to the proposed public urban park, ensuring passive surveillance of the open spaces, and addressing level changes across the site.*
- *Design, Layout and Unit Mix – further consideration of documents as they relate to the layout of the proposed development particular in relation to the 12 criteria set out in the Urban Design Manual which accompanies the Sustainable Residential Development in Urban Areas Guidelines and also reference to the Design Manual for Urban Roads and Streets.*

During 2018 and 2019 the scheme has been fundamentally redesigned. Additional lands to the south and east have been acquired by the applicant such that a footpath and cycle path from the proposed development can now be proposed down the Ballyhooly Road to connect to the existing footpath network. We note that the lands south of the planning application site are being reserved free from development so as to afford the City Council design flexibility in the delivery of LAP road objective NE-U-06 – the Mayfield Kilbarry Link Road Objective.

Another formal Section 247 meeting was held with Cork County Council on 23<sup>rd</sup> August 2018. At this meeting the Council noted that the National Guidelines require densities of 35-50 units per hectare in locations of this nature, and that densities should not fall below 30 units per hectare. The Council further noted that the sites topography may militate against achieving a higher density and that the density proposed complies with the LAP zoning and is similar to the recently permitted scheme at Ballinglanna, Glanmire, which has similar characteristics.

A further Section 5 Consultation Meeting with ABP and Cork City and County Councils took place on 11<sup>th</sup> June 2019. The scheme presented to the Board at this meeting had evolved to reflect feedback from the previous tripartite meeting and the Section 247 meeting with Cork County Council on 23<sup>rd</sup> August 2018.

The road alignment proposed followed a more circuitous, less steep route, than that proposed in ABP ref. PL 04.300557, working with the local topography and reducing cut and fill. Six neighbourhood clusters were proposed with a broad range of unit types and sizes. The density proposed achieved 35+ units per hectare. The Board's Pre-App Opinion stated that *'the documents submitted with the request to enter into consultations require further consideration and amendment to constitute a reasonable basis for an application for strategic housing development.'* The issues raised by ABP in their opinion are set out under the following headings:

- Timing and phasing
- Site topography
- Water and waste water infrastructure
- Surface water management and flooding
- Traffic and Transportation
- Residential Density
- Public Open Space
- Design, Layout and Unit Mix

A response to the Boards Opinion can be found in the accompanying Statement of Response to ABP Opinion prepared by Longview Estates Ltd. The scheme has been subject to further amendments following the Section 5 consultation meeting. The layout now proposed has taken into account all environmental effects raised with respect to previous design alternatives and within the Board's Opinion, and provides for a development that has been optimised to amplify positive environmental effects whilst reducing negative environmental impacts wherever possible.

In addition to the meetings and engagement with the Local Authorities and An Bord Pleanála, there has been engagement with Statutory Consultees and ongoing engagement with residents in the area. Statutory Consultees were issued letter with regard to the proposed development in October 2017 and May 2019.

Significant engagement has occurred with Irish Water during the design development of the proposed development. Critically, a PWSA (Project Works Service Agreement) has been entered into with Irish Water whereby the Applicant is



funding a significant portion of network improvements by Irish Water to both service the Site and to unlock other lands in the area; using a solution that has, prior to now, not been possible. This will address any foul water infrastructure needs that existed in the area for Phase One development, as defined under the Local Area Plan provisions for the Ballyvolane Urban Expansion Area (0-1175 Houses). The infrastructure will also allow for the wider Ballyvolane greater area to be opened up and serviced. At present, the development potential within the expansion area, without additional foul infrastructure, is very limited as both funding and delivery of this infrastructure requires a significant number of housing units to be consented to allow it to take place. The proposed development is fundamental in terms of front loading this infrastructure to unlock the development potential of this expansion area.

Engagement has occurred between the applicant and ESB Networks in relation to the proposed undergrounding of the 38kv line and correspondence has been enclosed at Appendix C. ESB networks have issued the proposed line diversion for the purpose of the proposed planning application and this is also included at Appendix C.

Please refer to Chapter 1 of the EIAR for further detail on consultation that has occurred with respect to the proposed development. Copies of various consultation records are attached at Appendix C of this report.

## **5.0 Planning Policy Context**

A standalone Statement of Consistency with the relevant planning policies has been prepared and is enclosed with this planning application. This Section of the report provides an overview of the relevant planning policies:

### **5.1 Local Planning Policies**

#### **Cork County Development Plan 2014**

The site forms part of the Northern Environs within the County Metropolitan Strategic Planning Area, which as stated at paragraph 2.2.22 of the Development Plan is *“the main engine of population and employment growth for the region”*.

The North Environs of Cork City are identified as being an important part of the Cork Gateway. The strategic aims for the Cork Gateway are set out in Objective CS 3-1 *Network of Settlements* which for the City Environs (North and South) seeks:

*“Growth in population and employment so that the Cork Gateway can compete efficiently for investment and jobs. Develop to complement and consolidate the development of the city as a whole and providing enhanced potential to rebalance the City through new development in the north”*.

Objective CS 4-1 of the Development Plan prioritises certain locations to accommodate the planned population growth, including the North Environs, and seeks to:

*“(g) Develop the Cork City Environs so that they complement the City as a whole. In the south, priority should be given to consolidating the rapid growth that has occurred in recent years by the provision of services, social infrastructure and recreation facilities to meet the needs of the*

*population. The North Environs will play a major role in the rebalancing of the City in terms of future population and employment growth.*

*(n) In the Cork Gateway, development to provide the homes and jobs that are necessary to serve the planned population will be prioritised in the following locations, Carrigaline (Shannon Park), Middleton (Waterrock) and Carrigtwohill (north of the railway), Ballincollig (Maglin), **North Environs (Ballyvolane)**, Glanmire (Dunkettle), Blarney (Stoneview), Monard and Cobh. Details of the proposed development will be set out in Master Plan studies and Local Area Plans as appropriate”.*

The County Development Plan sets a population target of 10,719 for 2022 which equates to 2,281 new units in the period 2011-2022.

The Development Plan sets out a number of policies relevant to the creation of sustainable communities, including Objective HOU 3-1 which has regard to the provisions of the Guidelines on Sustainable Residential Development in Urban Areas and the accompanying Urban Design Manual and seeks to:

*“(b) Promote development which prioritises and facilitates walking, cycling and public transport use, both within individual development’s and in the wider context of linking developments together and providing connections to the wider area, existing facilities and public transport nodes such as bus and rail stops; and*

*(c) Following the approach in chapter 10 of the plan, ensure that urban footpaths and public lighting are provided connecting all residential developments to the existing network of footpaths in an area and that the works required to give effect to this objective are identified early in the planning process to ensure such infrastructure is delivered in tandem with the occupation”.*

Policy Objective HOU 3-2 relates to urban design and states that the Council will have regard to Guidelines including the Sustainable Residential Development in Urban Areas and the accompanying Urban Design Manual and the Design Manual for Urban Roads and Streets (DMURS). The proposed development has been designed in accordance with these national planning guidance documents.

Policy HOU 3-2 also requires the submission of a design statement with all residential development applications. A detailed Design Statement prepared by Horgan Carroll Architects accompanies this application.

Objective HOU 3-3 aims to secure residential developments which provide a mix of house types and sizes to meet the needs of a range of households and to provide for mixed communities and tenures:

*“(a) Secure the development of a mix of house types and sizes throughout the County as a whole to meet the needs of the likely future population in accordance with the guidance set out in the Joint Housing Strategy and the Guidelines on Sustainable Residential Development in Urban Areas.*

*(b) Require the submission of a Statement of Housing Mix with all applications for multi-unit residential development in order to facilitate the proper evaluation of the proposal relative to this objective”.*

The proposed development comprises a wide range of house types and sizes to ensure the creation of a mixed community. Further detail is provided below and in the accompanying Design Statement.

The County Council’s approach to density is set out in Section 3.4 of the County Development Plan which indicates that the achievement of higher densities is primarily dependent on the proximity to public transport. It is considered that in many locations, the frequency of public transport services, particularly during peak hours, is currently below the level appropriate to the general application of higher densities.

On this basis and the fact that the Councils approach is to encourage a greater mix of house types on zoned land in the County Metropolitan Area in order to help moderate the future rate of population growth in rural areas, the Council takes a more flexible approach to density in this Development Plan. The Development Plan Objective HOU 4-1 sets out the density categories for zoned lands. The planning application site is predominantly zoned for Medium B residential development. The proposed development achieves a density of 35.7 units per hectare. The justification for the density proposed is set out below.

Development Plan Objective HOU 5-1 requires that 14% of all new residential developments to be made available for social housing. It is noted that this objective has been superseded by the requirements of the amended Planning and Development Regulations, pursuant to Part V, Section 96 of the Planning and Development Act 2000 (as amended), which requires that 10% of units are provided for social and affordable housing.

It is proposed to transfer 10% of units to meet this requirement and these units have already been agreed with Mr Brian Geaney and Ms Alison O’Rourke of Cork City Council Housing Department. A total of 75 Part V units are proposed comprising 1 no. 4-bed, 40 no. 3-bed and 34 no. 2-bed. The Part V units are pepper potted throughout the scheme. The Part V proposals have been agreed in principle with Cork City Council in advance of this application for formal agreement post planning approval.

Objective SC 1-1 requires the provision of social and community infrastructure as follows:

*“(a) Support the provision of social and community facilities which meet the current and future needs of the entire population;*

*(b) Secure lands for social and community facilities and encourage the provision of facilities suitable for intergenerational activities, which are accessible to all members of the community, through initiatives in partnership with community groups and sporting organisations”.*

Objective SC 5-2 seeks the quality provision of public open space in accordance with the standards of the Cork County Recreation and Amenity Policy and the Guidelines on Sustainable Residential Development in Urban Areas and the Making Places Design Guide for Residential Estates. This objective also seeks to:

*“(b) Promote the provision of high quality, accessible and suitably prioritised areas of public open space and promote linking of new open spaces with existing spaces to form a green infrastructure network”.*

It should be noted that Cork County Council published a revised interim approach to the implementation of the Council Recreation and Amenity policy. This interim approach applies to the provisions of the policy as they relate to the provision of recreational infrastructure within new housing development - Policy Objective 7 and associated Appendix A of the policy. The following provisions of the interim policy relate to the proposed development:

For schemes of 100 units and greater: For these schemes developers will be expected to make greater provision for on-site recreational facilities to cater for the needs of the development including play areas for young children and facilities for older age groups, which may typically include some or all of the following:

- Neighbourhood/Local Play Areas
- District Play Areas/Ball Courts/Multi Use Games Areas (MUGAs)
- Recreational walks/jogging routes
- Alternative facilities, other than those outlined, can be considered for substitution for the items listed. The acceptability or otherwise of substitute facilities shall be at the discretion of the Planning Authority.

Table 1 of the interim policy sets out the indicative thresholds for recreational facilities on schemes of 100 houses or greater.

**Table 2** Interim Recreation and Amenity Policy Table 1

Table 1: Indicative thresholds for recreational facilities on schemes of 100 houses or greater	
Type of Facility	Indicative threshold
Neighbourhood play areas	1 per 100 units
Local play areas	1 per 100 units
Recreational walks/jogging routes	May be considered where they can be suitably integrated into the design and are of sufficient length as to provide a useable route, or enhance connectivity to adjoining residential areas/nearby recreational infrastructure/ local services.
District Play Areas/Ball Courts	Suitable in larger schemes / sites where they can be appropriately sited – 250 houses minimum.
Multi Use Games Areas (MUGAs)	Suitable in larger schemes / sites where they can be appropriately sited – 250 houses minimum.

Objective TM 2-2 of the Development Plan aims to ensure that development in urban areas, towns and villages is well located, permeable and prioritises walking, cycling and access to public transport and other important amenities.

With regard to lands zoned for residential Objective ZU 3-2 states the following:

*“(a) Promote development mainly for housing, associated open space, community uses and, only where an acceptable standard of amenity can be maintained, a limited range of other uses that support the overall residential function of the area”.*

Appendix D of the Development Plan sets out the maximum parking and cycling standards that are applied in the County. In the North Environs, a maximum of 2 spaces are allowed per dwelling and 1.25 spaces per apartment; 1 space per 3 staff and 1 space per 10 children for crèches; 1 space per 50 sq.m for cultural, community and recreational buildings; 1 space per 20 sq.m for convenience retail and 2 per consulting room, 1 space per doctor/ consultant and 1 space per 3 nursing and ancillary staff for clinics and group medical practices.

Table 7.1 of the Development Plan – Retail Network/ Hierarchy and Objectives states that *“local retail facilities provide a valuable role in the communities they serve”.*

The site is located within a High Value Landscape area. As stated at paragraph 13.6.9 of the Development Plan within these High Value Landscapes care will be needed *“to successfully locate large scale developments without them becoming unduly obtrusive. Therefore, the location, siting and design of large scale developments within these areas will need careful consideration and any such developments should generally be supported by an assessment including a visual impact assessment...”*

### **Cobh Municipal District Local Area Plan 2017**

The proposed development is located within the area of the Cobh Municipal District Local Area Plan 2017. The vision for the Cork City North Environs to 2020 as set out in the LAP is:

*“To re-invigorate the northern suburbs of the city, within the County area, as a significant location for future residential development. This will require a planned major mixed use development at Ballyvolane coordinated with substantial infrastructure investment, the provision of enhanced community and recreational facilities and public transport accessibility, with the aim of rebalancing the provision of services more equitably throughout the city”.*

The lands are within the Ballyvolane Urban Expansion Area (UEA) which was identified in the Cork Area Strategic Plan 2008 Update as a significant growth location. A masterplan was required in the 2011 Blarney LAP to guide the development of the area and a special policy objective was identified to provide for a minimum of 2,337 and up to 3,600 dwellings through a phased programme of development.

This requirement for a Masterplan was omitted in the 2017 LAP as the lands were zoned. This LAP states that most of the expected growth of population for the North Environs of 10,719 persons will occur in the UEA. The LAP states that the number of units now proposed is approximately 3,000 and a total of 3 phases are recommended for the phased delivery of housing in tandem with the key infrastructure and facilities and amenities that are required to serve the area.

As noted the approach to the zoning of the subject lands has changed since the 2011 LAP. Under the current LAP, the X-01 special policy area has been divided into smaller parcels of specific zonings.

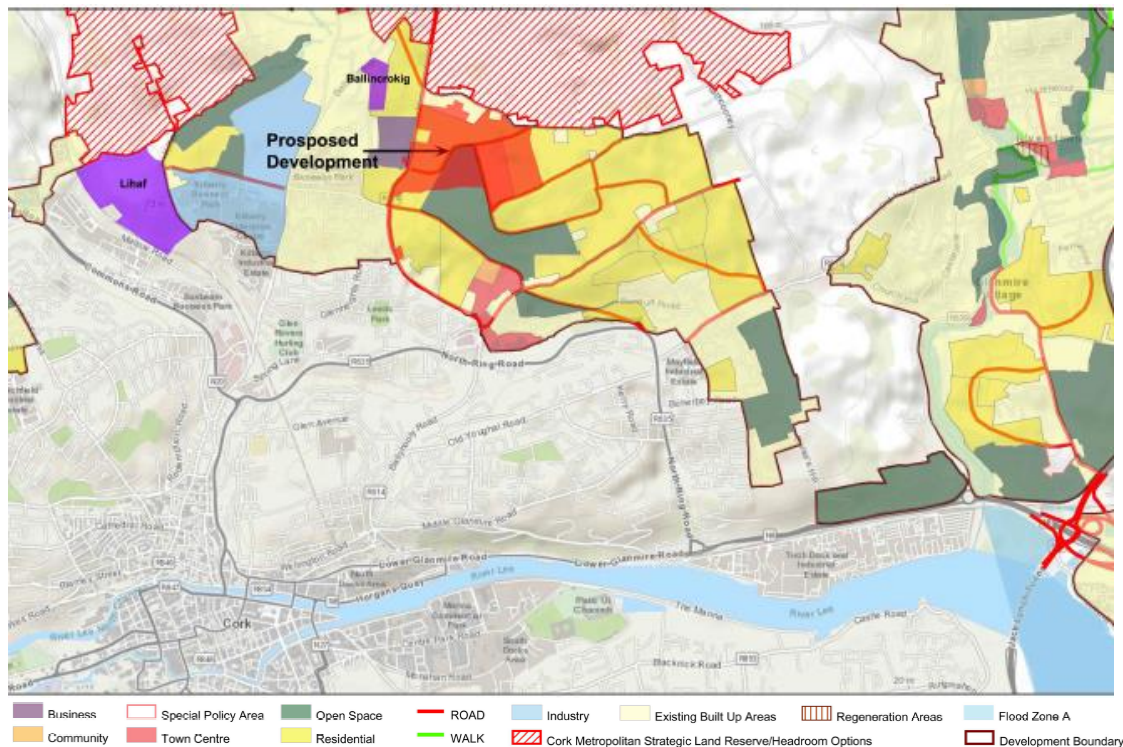


Under this revised approach, the subject lands are zoned as NE-R-08 and NE-R-09 for Medium B residential development; NE-C-01 for proposed primary and secondary school campus with playing pitches; NE-O-04 for open space for public recreation as an urban park. The amenity parkland should provide passive amenity for residents of the North Eastern suburbs and the site. It should also contain the necessary walkways and cycle-ways for accessibility between residential, business, retail and community uses and NE-U-03 for service road within the Ballyvolane Urban Expansion Area. The lands on which the Ballyhooly Road improvements (those proposed as part of this application) and the Irish Water Pumping Station, are located, is zoned NE-R-16 for Medium B residential development.

The LAP recognises that there are a number of constraints to development within the UEA including the road infrastructure, water services infrastructure, flooding, archaeological sites and the existing transmission lines. These are addressed in further detail in Section 6 of this report.

The key principles in considering the sustainable development of the site are summarised as effective connectivity (walking and cycling) to the town centre, educational and employment centres; high levels of modal shift; appropriate housing mix; housing density that encourages more efficient land use and infrastructure investment patterns and unique, innovative and distinctive house design with high quality finishes and materials.

**Figure 5** Proposed Development location within North Environs Zoning Map



The LAP sets out a number of general development objectives for the North Environs. We note in particular:

- NE-GO-01** – it is the aim of the Urban Expansion Area to achieve a high quality housing development consisting of a wide mix of house types and design to provide a choice of housing suitable to all age groups and persons at different stages of the housing cycle. It is envisaged that up to 3,000+ units can be accommodated on the site, however having regard to the steep topography in parts of the site the number of units may be less. It is noted that higher densities will be promoted closer to Ballyvolane Crossroads, and the City boundary to avail of a more frequent and high quality bus service.
- NE-GO-02** – In order to secure the sustainable population growth and supporting development proposed in NE--GO--01, appropriate and sustainable water and waste water infrastructure that will secure the objectives of the relevant River Basin Management Plan must be provided and be operational in advance of the commencement of any discharges from the development. Waste water infrastructure must be capable of treating discharges to ensure that water quality in the receiving harbour does not fall below legally required levels.
- NE-GO-03** – the design of residential development should be fine grained with houses of a particular type clustered in quite small groups avoiding continuous rows of development. House designs and layouts should be responsive to localised variations in their environment – including topography, orientation, retained features and recreational infrastructure. Avoid use of conventional house design with substantial use of retaining walls on sloping contours, house design should utilise the level difference within the site to an advantage.

- **NE-GO-04** – Construct a permeable built form with green routes to facilitate cycling and pedestrian movements within the site, with dedicated routes in locations with a shallower gradient. Routes should be created within the urban park to link phases of development and provide direct access to the educational facilities and improve permeability through the site. The route of the principal cycle-way shall link up with the Cork Cycle Network Plan 2017, a joint venture between City and County Councils.

The LAP sets out an indicative development programme for strategic infrastructure and service requirements for the UEA. Table 3.4.2 sets out the strategic infrastructure and service requirements that are required prior to commencement of development. These are addressed in the accompanying Statement of Consistency. Table 3.4.3 of the LAP sets out the indicative development programme for strategic infrastructure and service requirements of phase 1 – 0-1175 no. units. The proposed development will fall within phase 1.

<b>Table 3.4.3 of LAP Indicative Development Programme Ballyvolane</b>		
<b>Phase 1: 0-1175 units</b>		<b>Response</b>
<i>Water Services</i>	<ul style="list-style-type: none"> <li>• Provision of water supply reservoir (Irish Water)</li> <li>• Provision of capacity for waste water treatment and collection (as required) (Irish Water)</li> <li>• Implementation of SuDS Study recommendations which may include site specific arrangements</li> </ul>	<p>Irish Water has carried out a review of Water Supply Infrastructure having regard to the future residential development proposed and possible in the Ballyvolane expansion area. Irish Water are satisfied that there is sufficient water supply capacity to accommodate the phased expansion of the area. The water supply will be delivered on a phased basis but will require the extension of the services by approx. 780 m; from a point to the west on Dublin Hill by way of a 250 mm main extension.</p> <p>Phase 1 will be deliverable on the existing services network locally but the additional infrastructure will be required for phase 2 onwards.</p> <p>The proposal will provide for the provision of an Irish Water pumping station that will provide services for the site under an agreed Project Works Service Agreement (PWSA) with Irish Water. This infrastructure, provided through this application, will put in place services that also serve the residual zoned lands both in and out of the application area.</p> <p>On site surface water drainage infrastructure is provided including connection to surface water drainage</p>

		watercourses along the Ballyhooly Road on lands that are in the ownership of the applicant. Drainage rates will, though a combination of attenuation and on site infiltration be less than current greenfield run off rates.
<i>Transport</i>	<ul style="list-style-type: none"> <li>Identify and commence planning of measures to upgrade existing Northern Relief Road R635 and bus corridors into the City Centre in conjunction with City Council and NTA upgrade the Ballyhooly road, provide for pedestrians and cyclists</li> <li>Identify and commence planning of the upgrading of the Ballyhooly road, to provide for pedestrians and cyclists</li> <li>Plan and provide for the construction of a 2.2km Link Road from Mayfield to Kilbarry</li> <li>Plan and provide for the implementation of cycling and walking network within the site and on the Ballyhooly road</li> </ul>	<p>The proposed development commences the upgrading of the Ballyhooly Road for pedestrians and cyclists as it proposes a pedestrian and cycle path along the boundary of the site down the Ballyhooly Road to connect to the existing footpath network. This will provide a safe and convenient route for pedestrians and cyclists. The proposed development also includes for the signalisation of the Lower Dublin Hill/ Ballyhooly Road junction that will significantly improve the environment for pedestrians and cyclists on the Ballyhooly Road.</p> <p>The proposed development allows for the provision of the 2.2km Link Road from Mayfield to Kilbarry. Part of this proposed Link Road runs through the applicant's site. However, the proposed development does not compromise the provision of this Link Road in any way nor is the proposed development reliant on its development. The applicant as a commitment to Cork City Council has reserved this land free of development until the final alignment of the Link Road is determined so as not to compromise this transport infrastructure in any way.</p> <p>As noted the proposed development includes for a number of pedestrian and cycle routes that will facilitate safe and convenient navigation of the site and pedestrian and cycle paths are also proposed on the Ballyhooly Road to address the existing deficiency in the walking and cycling network here.</p>
<i>Open Space</i>	<ul style="list-style-type: none"> <li>Partial provision of the 20 hectare public park</li> <li>Completion of</li> </ul>	The proposed development includes 3 hectares of the proposed public park. The proposed development includes for a number of open spaces throughout. There are range of different types of open spaces

	<p>planning and design for the internal open space</p> <ul style="list-style-type: none"> <li>Planting of passive open space as part of overall landscaping strategy</li> </ul>	<p>provided to meet the needs of all ages. The siting and design of open spaces and how they are connected has been carefully considered within the sites Landscape Strategy. Please see Landscape Report enclosed with this planning application for further details.</p> <p>In addition to useable open spaces there are areas of the site that are not useable due to the sites challenging topography. The proposed development includes semi native shrub planting on the site embankments. The proposed development also includes passive open space in the form of woodland planting. Please refer to the Cunnane Stratton Reynolds Landscape Strategy for further details on passive open spaces within the development.</p>
<i>Retail</i>	<ul style="list-style-type: none"> <li>Plan and provide 10,000 sq.m of retail/ commercial floor space and associated car parking (which may include multi-storey) as a district centre to serve the development</li> </ul>	<p>The proposed development site is not within the District Centre Zoning. However a small element of retail will be included in the local centre to serve local needs. The local centre will include a crèche, 2 no. retail units, a doctors surgery and community use to support and serve local needs.</p>
<i>Education</i>	<ul style="list-style-type: none"> <li>Plan and make provision for a primary school (Dept of Education) on the lands reserved for a 16 classroom primary school adjacent to the large open space. The site should be procured by the Dept of Education</li> </ul>	<p>While no school is being proposed as part of this application, the proposed development will not compromise the provision of schools on the lands zoned for this use in any way. In fact it will open up the school lands with the proposed distributor road. The Department have confirmed that there is currently no requirement for a school in Ballyvolane. The Department will submit a planning application for the school at such a time in the future if it is deemed necessary.</p>
<i>Community Facilities</i>	<ul style="list-style-type: none"> <li>Childcare facilities, full time and sessional</li> <li>Provision of a health centre</li> <li>Provision of a community building</li> </ul>	<p>In accordance with the indicative phase 1 set out in the LAP, the proposal includes a crèche which is considered to be sufficiently sized by the Cork City Childcare Committee to cater for the demand that the proposed development is likely to</p>

	<ul style="list-style-type: none"> <li>• Fulfill the requirements of the recreational and amenity strategy</li> </ul>	<p>generate.</p> <p>A doctor's surgery is proposed within the local centre.</p> <p>The proposed development also includes a community building at the local centre to serve the proposed development.</p> <p>In June 2019 Cork County Council introduced an interim approach to the Councils Recreation and Amenity Policy. Pending a full review of the Recreation and Amenity Policy, this interim approach applies only to provisions of the policy as they relate to the provision of recreational infrastructure within new housing development. The proposed development is entirely compliant with the interim recreation and amenity policy. Further details are set out later in this report.</p>
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## 5.2 Regional Planning Policy

### The Southern Region Assembly: Draft Regional Spatial and Economic Strategy (RSES)

The Draft RSES sets out a 12 year strategic development framework for the Southern Region. It establishes a broad framework for development that is based on the NPF and sets out the vision for the Region's future. The RSES also identifies the overall framework for development which are set out in Metropolitan Area Strategic Plans (MASPs).

The Cork MASP positions Cork as the principle complementary location to Dublin. The MASP sets more ambitious population targets for Cork City and suburbs than the NPF with an estimated growth of 50,000 to 2026, 75,000 to 2031 and a population target of 283,669 for 2031. This reflects the constantly changing population projections as indicated in the NPF roadmap.

**Figure 6** Draft RSES Population Targets for Cork

Location	2016 Base Year	Growth to 2026	Growth to 2031	Population Target 2031
Cork City and Suburbs	208,669	50,000	75,000	283,669
Rest of Cork Met. Area	95,500	20,281	29,657	125,157
Total Metropolitan Area	304,169	70,281	104,657	408,826
Balance of Cork County	238,699	25,739	36,695	275,394

The Cork MASP identifies a number of strategic housing locations including the Ballyvolane UEA which it states has an estimated yield of 3,600 units. – *“the northern environs will be reinvigorated through sustainable development of mixed use development in Ballyvolane, complementing the North Blackpool Local Area Plan”*. It is noted that this is subject to specific infrastructure priorities for holistic infrastructure for metropolitan areas including:

- *Ballyvolane Urban Expansion Area phased infrastructure packages;*
- *Waste Water connections to Carrigrennan;*
- *Drinking Water supply infrastructure (new trunk mains, off site reservoir and pump station required);*
- *Sustainable Urban Drainage Strategy;*
- *Various Local Road Improvements (Delivery of Cork Northern Distributor Road and North East Orbital Road – access for residential lands and public transport infrastructure for Ballyvolane)”*.

The draft RSES supports infrastructure led planning (RPO 167) for future population and jobs growth in the region ensuring that social infrastructure are developed in tandem with population and jobs growth reflecting the RSES settlement strategy.

The draft Plan also promotes the 10 minute city and town concept (RPO 168) whereby a range of community facilities and services are accessible in short walking and cycle timeframes from homes or are accessible by high quality public transport services by connecting people to larger scaled settlements delivering these services.

## **5.2 National Planning Policy**

### **Project Ireland 2040 National Planning Framework (NPF)**

The National Planning Framework 2040 states that between 2018 and 2040, an average output of at least 25,000 new homes will need to be provided in Ireland every year to meet people’s needs for well-located and affordable housing. In the cities, the Housing Agency has identified an aggregate need for at least 45,000 new homes in Dublin, Cork, Limerick, Galway and Waterford up to 2020.

In the longer term, there will be a need for provision of at least 275,000 new homes in the cities, with half of these located in already built up areas. The growth of the regions as a counterbalance to Dublin is a key objective of the NPF. Specifically, NPF Objective 1b which estimates an additional 340,000-380,000 people in the Southern Region by 2040.

The NPF positions Cork as an emerging medium sized European city and places the emphasis of growth on the CASP study area. The focus of the NPF is on balanced, compact and connected growth and the plan aims to influence the location of new housing and future population growth. This is reflected in Policy Objective 2a which states a target of half (50%) of future population and employment growth will be focused in the existing five cities and their suburbs and Objective 3b which seeks to deliver at least half (50%) of all new homes that are targeted in the five cities and suburbs of Dublin, Cork, Limerick, Galway and Waterford, within their existing built-up footprints.

The NPF sets growth targets for Cork City and suburbs in the region of 105,000-125,000 and a minimum population of 314,000 by 2040. This will require a population growth rate of 50-60%. Other relevant national policies include Objective 4 which seeks to *“ensure the creation of attractive, liveable, well designed, high*



*quality urban places that are home to diverse and integrated communities that enjoy a high quality of life and well-being”.*

National Policy Objective 11 goes on to states that “*in meeting urban development requirements, there will be a presumption in favour of development that can encourage more people and generate more jobs and activity within existing cities, towns and villages, subject to development meeting appropriate planning standards and achieving targeted growth”.*

The NPF identified key growth enablers for Cork which include progressing the sustainable development of new greenfield areas for housing, especially those on public transport corridors.

In terms of the location of houses, NPF Objective 33 prioritises the provision of new homes at locations that can support sustainable development and at an appropriate scale of provision relative to location. The Plan proceeds to state that Ireland’s future homes will be located in places which support growth, innovation and the efficient provision of infrastructure, are accessible to a range of local services, can encourage the use of public transport, walking and cycling, and help tackle climate change.

The NPF recognises the importance of resilience in housing and Objective 34 specifically supports the provision of lifetime adaptable homes that can accommodate the changing needs of a household over time.

## **6.0 Planning History**

There is no recent planning history associated with the proposed development site. Planning applications on adjoining lands have generally been for single dwellings, minor household alterations and extensions and minor agricultural developments.

Residential development in Ballyvolane and in the Northern Environs generally has been limited over the past number of years. The only recent large scale residential development permitted in the area is Cork County Council ref. 17/6781 for 74 no. units to the north west of this site at Ballincroig.

## **7.0 Planning Assessment**

The key planning matters pertaining to the proposed development are assessed in the following section. They key matters considered include:

- Compliance with Planning Policies
- Density and Unit Mix
- Access and Connectivity
- Traffic and Transport
- Detailed Design Approach
- Recreation, Amenity and Open Space
- Landscape and Visual Impact
- Childcare and Education
- Services Infrastructure
- Archaeology
- Ecology
- Natura Impact Assessment
- Environmental Impact Assessment (EIAR)

- Part V

## 7.1 Compliance with Planning Policies

A Statement of Consistency with the relevant planning policies has been included with this application. This report considers the compliance of the proposed development against the relevant local, regional and national planning policies. The report concludes that the proposed development is consistent with the site specific zoning objectives for the relevant lands.

It is noted that the density proposed is above that which has been identified for Medium B density in the Cork County Development Plan 2014. However, this higher density is in accordance with the Board's Opinion of February 2018 (ABP ref. PL 04.300557) and June 2019 (ABP ref. PL04 .304350) and emerging precedents from the SHD process.

There is a clear and urgent need for housing in Ireland, particularly within the larger urban centres such as Cork. The pressing need for housing output is evident in the significant changes in planning policy which have been emerging including the National Planning Framework, the Draft Regional and Economic Strategy, Sustainable Urban Housing: Design Standards for New Apartments and the Urban Development and Building Heights Guidelines for Planning Authorities.

These recent planning policies and guidance have been focused on achieving significant housing output through increased densities on appropriate sites which are zoned and serviceable.

## 7.2 Density and Unit Mix

### *Density*

The site would be identified as 'Outer Suburban / Greenfield' as per the Sustainable Urban Housing Guidelines. These are sites which are defined as "*open lands on the periphery of cities or larger towns whose development will require the provision of new infrastructure, roads, sewers and ancillary social and commercial facilities, schools, shops, employment and community facilities*". The Guidelines encourage densities in the general range of 35-50 dwellings per hectare on such lands stating that development at net densities less than 30 dwellings per hectare should generally be discouraged in the interests of land efficiency.

As noted in the Planning Policy section, the approach of Cork County Council to density is more flexible. While the Development Plan recognises that increased densities are required for a sound return on infrastructure investment, it also recognises that a degree of flexibility for developers to adapt to market conditions and provide a range of house types must be possible. The Development Plan sets out three density categories.

In the 2017 LAP, the site includes land zoned as NE-R-08 and NE-R-09 for Medium B residential development. In the County Development Plan lands zoned as Medium B have a density range of between 12-25 dwellings per hectare and this density is stated as normally being applicable in smaller towns (less than 5,000 population), but can be applied in larger towns through LAP's where there is a requirement to broaden the range of house types.

The scheme previously presented to An Bord Pleanála under ref. PL 04.300557 had 457 no. units at a density of circa 18 units per hectare. Cork County Council were satisfied that this density complied with the zoning objective. In this scheme the density had been guided by the initial topographical survey for the lands and the LAP designations, in particular the distributor road through the site. At the outset, the proposal complied strictly to the distributor road alignment as shown on the North Environs zoning map in the 2017 LAP.

Early discussions with Cork County Council focused heavily on the position of this route and the feedback from the Council was that this alignment was intended to be indicative. In August 2017, ahead of the first formal S247 meeting in September 2017, the Council Roads Department requested that the design team consider a different design solution for the distributor road which digressed from the alignment illustrated in the LAP. Arising out of the S247 meeting and a subsequent meeting between members of the design team and Officers from the Council's Roads Department, the layout of the distributor road was amended.

In terms of design the layout originally submitted to the Board was also heavily influenced by the location. The design team were cognisant of the fact that this is a peripheral location, though less than 3km from the City Centre, it is a transition area that is currently rural in character. The density and house types were guided by this, particularly the need to prevent any adverse impacts on existing housing to the north of the site in accordance with the LAP which specifically states *“the pockets of existing one off houses will necessitate a buffer of low density housing to protect their residential amenity. The areas concerned are along the existing roads. The higher densities should be concentrated on the Western and Southern sections of the site with the lower densities to the East and medium density to the North subject to adequate consideration for the landscape and sloping nature of the site”*.

The feedback from the Board was that the proposal required further consideration and amendment including that the sites density should have regard to the national guidelines. The Opinion of the Board specifically stated:

*“Further consideration of documents as they relate to the residential density of the site. This consideration and justification should have regard to, inter alia, the minimum densities provided for in the Guidelines for Planning Authorities on Sustainable Residential Development in Urban Areas (including the associated Urban Design Manual). Particular regard should be had to the need to develop at a sufficiently high density to underpin the efficiency of existing or planned public transport services. The further consideration of this issue may require an amendment to the documents and/ or design proposals submitted relating to density and layout of the proposed development”*.

The scheme presented to the Board under ABP Ref. PL04 .304350 comprised a significant redesign providing for 753 no. units at a density of 35.3 units per hectare. As noted a significant redesign of the scheme has occurred and the proposal presently with the Board provides for 753 no units at a density of 35+ units per hectare. The density now proposed is 35.7 u/Ha.

**Table 3** Density of Individual Neighbourhoods

Neighborhood	Density	No. of Units
Neighbourhood 1	25.9 units/hectare	75
Neighbourhood 2	37.6 units/hectare	218
Neighbourhood 3	27.6 units/hectare	63
Neighbourhood 4	25.9 units/hectare	93
Neighbourhood 5	33.5 units/hectare	178
Neighbourhood 6	103.5 units/hectare	126

The density varies in each neighbourhood cluster in accordance with the context and location of that neighbourhood and specifically the steepness and ground conditions of that part of the site. The highest density is proposed in neighbourhood 6 where apartments are proposed close to public transport with the lower densities at neighbourhoods 1 and 3 where the prevailing site conditions are more challenging i.e. rising topography and the very high costs involved in addressing the ground conditions.

We note that the Board have taken site specific constraints into consideration in assessing other Strategic Housing Development applications including ref. ABP-300543-18 at Ballinglanna, Glanmire, Cork where the Board Inspector noted:

*“I consider that the net provision of 30.5 units/ ha is low for zoned and serviced land, particularly with regard to the strategic importance of the development site for the overall provision of residential development in Metropolitan Cork. However, given the site constraints including wayleaves, the roads objective, steeply sloping topography, proximity to the M8, the presence of Ballinglanna House, the necessity to retain and manage extensive wooded areas and the lack of public transport in the area, it is considered that the overall density is appropriate”.*

This proposal achieves an overall density of 35.7 unit per Ha Nett, the mix of house types and unit types is delivering a high density of rooms per Ha. National guidance on density makes reference, in general, to the position that *“Studies have indicated that whilst the land take of the ancillary facilities remains relatively constant, the greatest efficiency in land usage on such lands will be achieved by providing net residential densities in the general range of 35-50 dwellings per hectare and such densities (involving a variety of housing types where possible) should be encouraged generally. Development at net densities less than 30 dwellings per hectare should generally be discouraged in the interests of land efficiency, particularly on sites in excess of 0.5 hectares”.*

However, the Irish Guidance, is derived from UK guidance<sup>2</sup> which delves further into the discussion of density by also looking at habitable rooms Per Hectare. The table following is a Density Matrix extracted from the *“Urban Design Compendium Homes and Communities Agency UK”*. This reflected a more nuanced approach to density

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<sup>2</sup> Urban Design Compendium Homes and Communities Agency UK

than the raw calculation of housing density as a function of units per Ha in that it also considered Habitable Rooms per Ha. In total there are 3743 Habitable Rooms in the scheme which equates to a figure of 175 rooms per Hectare.

This table shows that 175 Rooms Per Ha is close to the suburban standard for “Sites Along Transport Corridors”. While the proposed development, on a unit per Ha basis is at 35.7 per Ha., due to the number of rooms and three storey nature of some houses, we achieve room numbers of 175 Per Ha. **This correlates with a density approx. 40 P/Ha Nett.**

**Figure 7** Extract from *Urban Design Compendium Homes and Communities Agency UK*

		Option 1	Option 2	Option 3
<b>Car Parking Provision</b>		<b>High</b> 2-1.5 spaces per unit	<b>Moderate</b> 1.5-1 space per unit	<b>Low</b> less than 1 space per unit
<b>Redominant Housing Type</b>		<b>Detached &amp; linked houses</b>	<b>Terraced houses &amp; flats</b>	<b>Mostly flats</b>
<b>Location</b>	<b>Setting</b>			
Site within Town Centre 'Ped-Shed' ↑ 6 ↓ 4	<b>Central</b>			240-1100 hr / ha 240-435 u / ha Ave. 2.7 hr / u
	<b>Urban</b>		200-450 hr / ha 55-175 u / ha Ave. 3.1 hr / u	450-700 hr / ha 165-275 u / ha Ave. 2.7 hr / u
	<b>Suburban</b>		240-250 hr / ha 35-60 u / ha Ave. 4.2 hr / u	250-350 hr / ha 80-120 u / ha Ave. 3.0 hr / u
Sites along Transport Corridors & Sites close to a Town Centre 'Ped-Shed' ↑ 3 ↓ 2	<b>Urban</b>		200-300 hr / ha 50-110 u / ha Ave. 3.7 hr / u	300-450 hr / ha 100-150 u / ha Ave. 3.0 hr / u
	<b>Suburban</b>	150-200 hr / ha 30-50 u / ha Ave. 4.6 hr / u	200-250 hr / ha 50-80 u / ha Ave. 3.8 hr / u	
Currently Remote Sites ↑ 2 ↓ 1	<b>Suburban</b>	150-200 hr / ha 30-65 u / ha Ave. 4.4 hr / u		

**Table 3.3 Density matrix**

Average densities are based on case studies analysed as part of the *Sustainable Residential Quality: Exploring the housing potential of large sites* research (LPAC, DETR, GOL, LT and HC, 2000)

The density now proposed has we consider balanced the requirements of sustainable development and will result in the creation of high quality homes and neighbourhoods, where people want to live and raise families.

The proposed density of 35+per hectare is entirely appropriate on this site and consistent with the relevant national guidance contained in the Sustainable Residential Development in Urban Areas Guidelines which state that “*development at net densities less than 30 dwellings per hectare should generally be discouraged in the interests of land efficiency*”.

As confirmed in the *Summary Construction Advice Report* prepared by MAC Construction Consultants enclosed with this application it is hard to appreciate the full extent of cost abnormal’s in delivering schemes of 35/ 35+ units per hectare on sites such as the lands at Lahardane when compared with flat sites. The report by

MAC Construction reflects the current market conditions and restrictions relating to the varied issues associated with a scheme of this scale, location, density and longevity. The report concludes that the scheme as currently proposed has been optimized as best possible to suit the site and its issues as well as the current market that exists.

### *Unit Mix*

With regard to unit mix, it the policy of Cork County Council (HOU 3-3) to have a mix of house types and sizes throughout County Cork to meet the needs of the population. There are 753 no. units proposed in a range of house and apartment types and sizes as set out in the tables below.

The unit mix for the proposed development has been influenced by factors including:

1. The nature of the existing housing stock in the area and the need to provide a choice of housing;
2. The need to provide housing that is suitable to all age groups and persons at different stages of the life cycle;
3. The existing social mix in the area;
4. The need to cater for a mixed community with a range of house types and sizes and social housing;
5. The saleability of different types of housing.

Unit sizes range from 41 sq.m to 168.7 sq.m and range from studio's to 4 no. bedroom units. Pursuant to Section 96 of the PDA Act 2000 (as amended) provision has been made for 10% Part V units which are "pepper potted" throughout the site. The considered mix of unit types and sizes will ensure choice, affordability and a better quality of life for the future population and the creation of a sustainable mixed tenure.

**Table 4** Proposed Unit Mix (Unit Type)

<b>Neighbourhood</b>	<b>Detached</b>	<b>Semi-detached</b>	<b>Terrace</b>	<b>Duplex</b>	<b>Apartments</b>
Neighbourhood 1	12	50	4	9	
Neighbourhood 2	20	60	75	36	27
Neighbourhood 3	8	50	5		
Neighbourhood 4	14	54	25		
Neighbourhood 5	13	64	77	24	
Neighbourhood 6					126
<b>Total</b>	<b>67</b>	<b>278</b>	<b>186</b>	<b>69</b>	<b>153</b>

The breakdown of units according to number of bedrooms per unit is as follows:

**Table 5** Proposed Unit Mix (Bedroom Numbers)

Type	No.
Studios	6
One Bedroom Units	42
Two Bedroom Units	184
Three Bedroom Units	431
Four Bedroom Units	90
<b>Total</b>	<b>753</b>

### 7.3 Access and Connectivity

The proposed development includes for two vehicular access points from the Ballyhooly Road and one to/ from the local road to the north of the site. As is required by LAP Objective NE-U-03, a service road is provided through the Longview lands from the Ballyhooly Road which will serve the proposed development and facilitate the development of other zoned lands in the UEA. This is the only route proposed within the LAP from the Ballyhooly Road that can facilitate west to east connections through the expansion area. The proposed development also partially provides service road NE-U-04 within the LAP. These service roads which traverse the proposed development will facilitate access to the future school site/ community lands and open up other residentially zoned lands within the UEA to the North East to help meet Cork's housing targets.

The lands are well located to enable connections to the City Centre and local facilities and services in Ballyvolane and Blackpool. The Ballyhooly Road is an important regional route which connects the site to the City Centre through Dillon's Cross and St Luke's and to the Northern environs of the city including Upper Glanmire, Watergrasshill, and Carrignavar through Whites Cross.

The existing closest bus stops are located at Brookwood (300m from the proposed main site entrance) and Kempton Park which both serve the 207. The 207 runs from Ballyvolane to Donnybrook via Cork City Centre with a terminus at Glenheights Park on Glenheights Road and a terminus at Scairt Cross, Donnybrook. The route serves Glen Rovers Hurling Club, Ballyvolane Business Park, Ballyvolane Shopping Centre, Cork City Centre and Douglas. Services depart every 30 minutes from Glenheights between 07.10 to 23.00 Monday to Saturday and on Sundays from 09.30 to 23.00.

The outbound route runs from Donnybrook to Ballyvolane through Patrick Street with the same frequency as the inbound route. The inbound route commences at Glenheights Park, travels via Ballyvolane Business Park, east along the North Ring Road, northeast along Ballyvolane Road past the shopping centre, then south along Ballyhooly Road through Dillon's Cross, St. Luke's and down Summerhill North, across Brian Boru Bridge to the bus station at Parnell Place before commencing its southbound part of the route.

As part of the Ballyvolane Strategic Transport Corridor (BSTC) significant improvements to Route 207 are proposed with the aim of decreasing journey times and enhancing public facilities. These include bus shelters and RTPi (Realtime Public Information) boards to be provided at bus stops along the corridor and bus priority at all signal-controlled junctions. With the provision of these facilities and other incentives as part of national policy, it is anticipated that a shift to public transport will occur over the construction phase of this scheme.



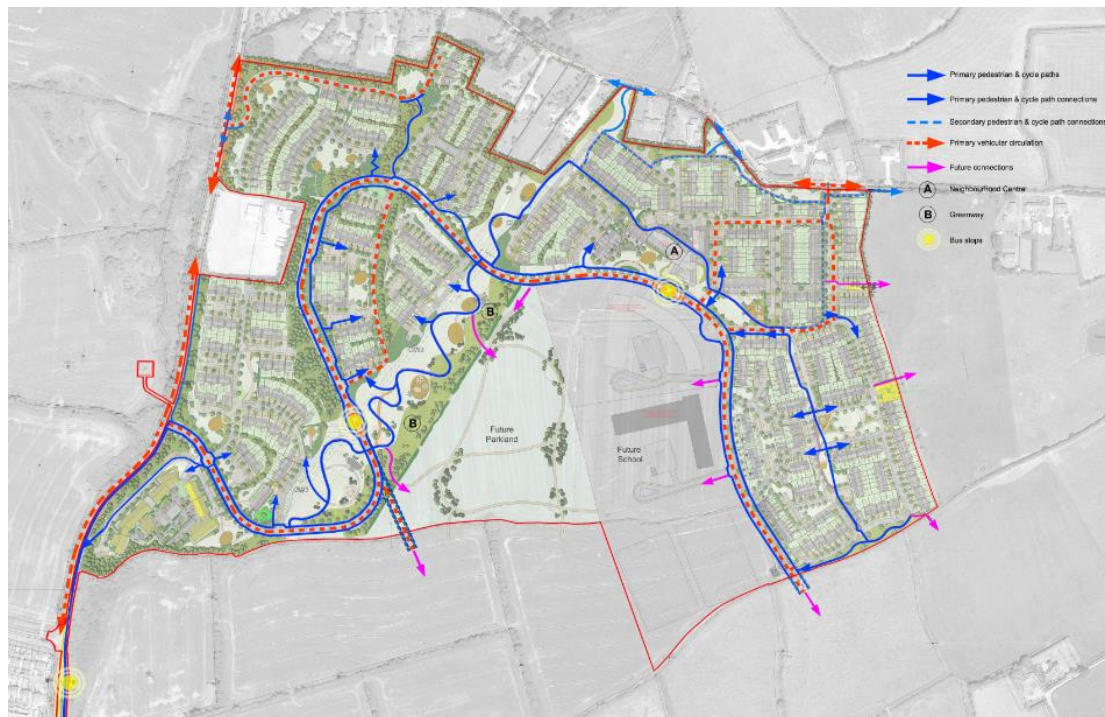
The proposed development includes a new bus stop on the Ballyhooly Road that is compatible with the BSTC scheme. Bus stops are also proposed on the distributor road to facilitate bus routes serving the expansion area. A temporary bus turning area is proposed as part of this planning application to facilitate the provision of a bus service until such time as the bus can continue throughout the expansion area when other lands are developed.

Significant pedestrian and cyclist infrastructure are proposed in this planning application to improve connectivity internally within the site, to surrounding areas, to Ballyvolane District Centre and the City Centre. Some 2.3km of new footpaths/ cycleways are proposed. Within the site a dedicated greenway will provide safe and convenient connections throughout the site between neighbourhoods, the park and local centre.

There is currently no footpath on the Ballyhooly Road north of Mervue Lawn. Improvements proposed as part of the development include for the provision of cycle lanes and a footpath to connect to the existing network to the south. It is proposed to have 2m wide cycle lanes to the east of the Ballyhooly Road for cyclists travelling to and from the city and a 2m wide footpath. This is in accordance with the Cork Cycle Network Plan which envisages a primary cycle route (route code CCN-U17) along the Ballyhooly Road from Glen Avenue to Mervue Lawn with the potential to be extended further north to serve other areas of the master plan if required.

It is also proposed to signalise the junction of Ballyhooly Road and Kilbarry Link Road which will facilitate access to schools and services to the west of Ballyhooly Road.

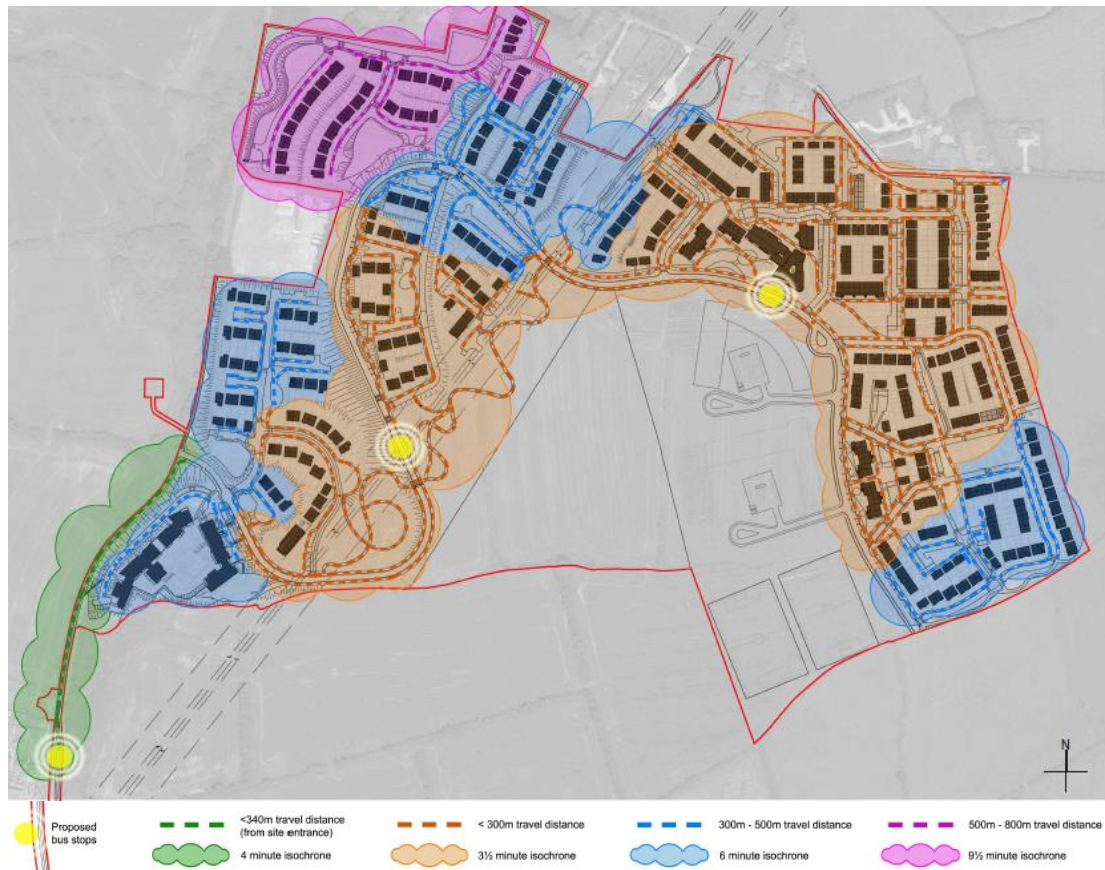
**Figure 8** Connections proposed



The isochrones connectivity maps prepared by Horgan Carroll Architects illustrate how the site will function within its overall context in terms of the accessibility to bus stops and the local centre.

Within the site, the local centre and park will act as focal points. All units will have short, safe and convenient access to these areas by foot or bicycle including through the greenway which will provide an attractive off road alternative means of moving within the site.

**Figure 9** Isochrone image showing times and distances to bus stops proposed



In terms of accessibility the proposed development has been designed to so that it can be accessed, understood and used to the greatest extent possible by all people, regardless of their age, size or disability. This includes public places in the built environment such as buildings, streets or spaces that the public have access to.

The Building Control (Amendment) (No.2) Regulations 2015 (S.I. No. 365 of 2015) BCAR, will be applicable to all residential apartments, dwelling houses, crèche, community centre, doctors surgery and retail units. Evidence of compliance with the Building Regulations will be provided to the Local Authority under this process. Please refer to the Universal Design Statement prepared by Horgan Carroll Architects that accompanies this planning application.

#### 7.4 Traffic and Transport

MHL Consulting Engineers have prepared a Traffic and Transport Assessment (TTA) for the EIAR included with this planning application. The scope of the TTA was agreed with Cork City Councils Traffic and Transportation Department. The TTA notes that on completion of the Ballyhooly Transport Corridor works, scheduled for the first quarter of 2023 and with the pedestrian, cycle and public transport infrastructure proposed in this development, an increase in modal shift of 33% would be expected to be delivered for the area. This increase in modal shift

has not been applied to existing traffic numbers when developing future year traffic models but was used in developing traffic generation from the site. In-line with the 2016 Census Data the existing modal shift rate of 12% has been assumed to remain stagnant up to the end of 2021. Thereafter, with the completion of the Ballyhooly Strategic Transport Corridor Scheme, this modal shift rate is expected to increase with the aim of achieving a rate of 45% by the end of 2025.

In summary the TTA has demonstrated the following:

- i. The proposed residential development is in accordance with the Local Area Plan and forms an important first part in the delivery of planned growth in the area.
- ii. A review of the existing roads network and collision data in the vicinity of the site indicates that there are no significant problems in relation to the current operation or safety of the identified junctions. Existing traffic congestion on the R635 North Ring Road during peak periods has already been reduced as a result of Phase I of the BSTC project being implemented. The future year traffic modelling carried out as part of this assessment demonstrates that with further junction improvements proposed as part of the next phase of public works, additional capacity at junctions is generated.
- iii. The proposed site layout is permeable to the roads network and is connected to existing pedestrian linkages to public transport offerings, schools, retail and amenity destinations.
- iv. The proposed new access arrangements are safe and suitable and are in accordance with the Design Manual for Roads & Bridges (DMRB) and the Design Manual for Urban Roads & Streets (DMURS).
- v. The site benefits from being in close proximity to regular transport provision, within walking distance of the site, which enables journeys throughout Cork City.
- vi. Junction mitigation measures proposed as part of this application include the signalisation of Junction 1: Ballyhooly Road/Kilbarry Link Road incorporating a toucan crossing to facilitate pedestrian/cycle connectivity to local bus stops, schools, church, sports grounds (soccer and GAA) and existing residential areas. The traffic modelling of this junction using LinSig software has demonstrated that the junction operates with spare capacity up to and including the completion of the development in 2029. The junction has been assessed to include for a pedestrian phase each and every cycle. The safety implication of this proposal is positive as the junction and its approaches will be traffic calmed and in addition the signalisation will regularise traffic flow entering the urban environment.
- vii. As part of the development proposal the inclusion of an off-road cycle and pedestrian facility from the development entrance to a proposed crossing of the Ballyhooly Road at Mervue Lawn, south of the site, is to be delivered. This will facilitate and encourage modal shift towards more sustainable modes of travel.
- viii. The cumulative impact of the development, inclusive of future growth background traffic and with proposed junction upgrades to be delivered as part of the BSTC project being in place from 2023 onwards, has been tested on the identified critical junctions, indicating that the proposed development

can be accommodated without the need for alternative routes (for example the Northern Orbital Route) as outlined in CMATS, being in place.

- ix. The completion of the aforementioned Northern Orbital Route (NOR), which will link from 'Tinker's Cross' (Junction of the R635 & North Ring Road) on the R635, North Ring Road, to the N20 will provide an alternative route for traffic currently using the R635 to bypass the city centre. This route is indicated to pass to the south of the development site, incorporating the 'Kilbarry Link Road' and will serve as a high-quality bus corridor (QBC) orbiting the city. Arterial links to the city centre, such as the Ballyvolane Strategic Transport Corridor, will provide connectivity inwards from this route. The NOR is identified in CMATS (Cork Metropolitan Area Transport Study) with an expected delivery date by 2031. On its completion traffic and travel patterns in this area will undergo significant change which has been included in the SWRM (NTA South West Regional Model) future year scenarios. Consequently, this TTA does not look beyond a 2029 Design Year.
- x. The proposed development is to be accessed via two priority junctions onto the R614 Ballyhooly Road as shown in the following Figure. These junctions have been assessed for development traffic and background traffic flows on the Ballyhooly Road and found to operate within capacity up to and beyond the design year.

The TTA concludes that the proposed development, in traffic and transportation terms is acceptable, and there are no traffic and transportation reasons that should prevent the Planning Authority from recommending approval of this application.

## 7.5 Detailed Design Approach

The proposed development has been designed in accordance with guidelines including the Sustainable Residential Development in Urban Areas Guidelines, the Urban Design Manual and the Design Manual for Urban Roads and Streets.

This planning application is accompanied by a Design Statement prepared by Horgan Carroll Architects which sets out the design strategy for the proposed development. It is expected that the development will be constructed over six phases and it includes 6 no. neighbourhoods. The layout of the neighbourhoods has been formed from the alignment of the distributor road which provides access from the Ballyhooly Road and runs through the site from west to east providing access to individual neighbourhoods, the lands zoned for a school campus and providing potential access to undeveloped lands to the south east of the site in the future.

The proposed greenway/ urban park and the local centre are distinct elements of the proposed development and will act as focal points and be recognisable features within the scheme. The houses will be 2 to 3 storeys, the duplex units 4 storeys and the apartments 4-6 storeys. The taller buildings are located centrally within the site close to the local centre and in neighbourhood 6 close to the bus stop on Ballyhooly Road.

### *Houses*

The houses are designed as two and three storey family dwellings, in detached, semi-detached and terraced buildings. There are 67 no. detached dwellings, 278 no. semi-detached dwellings and 186 no. terraced dwellings proposed. These are



spread throughout the site in each neighbourhood and will offer choice in accommodation to meet different family sizes and needs.

To allow for future adaptability the majority of dwellings have been designed to provide the option of an attic conversion or a single/ two storey extension to the rear as gardens are generally larger than current minimum requirements.

**Figure 10** Houses in Neighbourhood 1



In terms of energy efficiency, the proposed development has been designed to be compliant with The Building Regulations, Technical Guidance Document Part L, Conservation of Fuel and Energy – Dwellings, 2019. All residential units are designed to meet Near Zero Energy Buildings (NZEB) standards. Preliminary Dwelling Energy Assessment Procedures (DEAP) have been carried out and the results confirm that compliance with NZEB will be comfortably achieved. The assessments also set out the potential for further and future improvement in efficiencies of up to 30% with the addition of photo voltaic (PV) technologies. Houses and apartments will achieve A2 / A3 Building Energy Ratings with the potential for improvement to A1 with the addition of PV technology.

Houses are designed in excess of current energy efficiency standards and there is potential for the incorporation of photovoltaic panels and heat recovery systems to enhance further the efficiency of homes. Passive house principles have been applied in designing the scheme. The application on of these principles, has resulted in potentially 90% of all units having the primary, living room ie. kitchen, dining, living, availing of direct sunlight from the south east, south and south west. Such an arrangement provides passive solar gain to the most frequently occupied living space in the home, improving quality of living and directly reducing heating and lighting requirements and thus further reducing carbon dioxide emissions

## *Apartments*

The massing and scale of the proposed apartment buildings respond directly to the topography and positioning at focal points within the site. Neighbourhood 6 consists of the taller apartment blocks to respond to the steep slope of the area. The apartments along the Ballyhooly Road provide a distinctive gateway into the development and a focal point along the road. The form of the apartment blocks allow for an undercroft parking area and apartments on lower parts of the site to compensate for drops in levels. Neighbourhood 2 apartments are designed as a node or focal point within the site close to the local centre.

**Figure 11** Apartments proposed in Neighbourhood 6



## *Local Centre*

The local centre occupies a central location within Neighbourhood 2 of the site to ensure ease of access for all future residents. The local centre includes a public plaza area surrounded by apartments, a community building, crèche, 2 no. retail units and a doctor's surgery. The local centre will function as a social hub within the proposed development.



**Figure 12** Proposed Local Centre



A consistent aesthetic approach is adopted throughout the development, a contemporary architectural interpretation and material variations are incorporated to deliver variety and improve place making throughout the site. The material proposed are aimed at creating a visual aesthetic to enhance the local setting and surrounding vernacular with a more contemporary approach. The house types and palette of materials proposed has been carefully considered to provide a coherent and mixed variety to the overall development that will contribute to the locality.

The principles of the Design Manual for Urban Roads and Streets (DMURS) have been incorporated into the design of the development's roads, to clearly define a hierarchy which aims to improve legibility throughout the scheme. The proposed development has sought to:

- a) Assign higher priority to pedestrians and cyclists, without unduly compromising vehicle movement, in order to create secure, connected places that work for all members of the community.
- b) Create attractive places to live.
- c) Facilitate walking and cycling as alternative modes of transport (both in the layout of spaces and the provision of walks, cycle paths and ample cycle parking).
- d) Deprioritise the car, accommodating it where necessary (the distributor road) and controlling its design effect in the housing areas.

## 7.6 Recreation, Amenity and Open Space

The County Development Plan requires that all new residential developments have an adequate provision of open and amenity space to meet the needs of future residents. The Councils Recreation and Amenity Policy requires the provision of 12%-18% open space on sites. The proposed development provides well in excess



of this requirement with 32% open (including the greenway); 15.2% (within the neighbourhoods).

As noted pending a full review of the Recreation and Amenity Policy, Cork County Council published an 'Interim Approach to Implementation' of the policy in June 2019. *'This interim approach applies only to provisions of the policy as they relate to the provision of recreational infrastructure within new housing development – Policy Objective 7 and associated Appendix A of the policy.'* The proposed development is entirely consistent with this policy.

The recreation and Amenity provision has been central to the design development process for the development ensuring that facilities are readily accessible, functional and fully integrated as part of individual neighbourhoods and the wider landscape infrastructure of the site. The combined value of recreational and amenity facilities significantly influences the experience of place and the general quality of the residential environments through the site. Recreation and Amenity provision comprises a combination of formal and informal features including the following:

- 32,122.26 sq.m of green areas within the neighbourhoods
- 7 no. Local Play Areas
- 8 no. Neighbourhood Play Areas
- 3 Hectares of public park
- 1.6km of primary pedestrian/ cyclist greenway route through the site
- 1.5 km (along distributor road) and 0.6km (Neighbourhood 2) of secondary pedestrian/ cyclist greenway route
- An outdoor gym/ exercise facility
- 3 no. playgrounds with equipment
- Large kickabout space in the public park
- 2 no. amphitheatre structures
- A public plaza/ social space at the local centre
- Courtyard recreational space within Neighbourhood 6.

Please refer to Cunnane Stratton Reynolds Landscape Report for further details.

**Figure 13** Proposed Park Land



### 7.7 Landscape and Visual Impact

A detailed landscape masterplan prepared by Cunnane Stratton Reynolds accompanies this application. The landscape strategy for the site has been influenced by the following key design aims:

- Prioritisation of 'Place Making' in the evolution of the site layout to achieve a sense of place/distinctiveness throughout the site
- Provision of a planting framework across the site into which the proposed development will be integrated
- Prioritisation of pedestrian/cyclist movement through the site in line with DMURS principles
- The provision of safe, overlooked open spaces, routes and facilities throughout the site with access for all
- The creation of a hierarchy of accessible and overlooked open spaces in neighbourhoods with well-defined connectivity to the sites wider green infrastructure
- The promotion of a child centred approach, based, where appropriate, on 'home zone' principles that enable children of all ages to move freely and safely around their neighbourhood
- Communal space provision of multi-functional and flexible communal spaces
- The provision of legible routes and clear definition of space
- To optimise the experience of panoramic views along the wayleave greenway corridor
- To maximise the retention of existing hedgerows and to provide new planting measures which enhance the biodiversity potential of the site
- To ensure maintenance and management of the landscape infrastructure is manageable and promotes long term sustainability

The specification of materials and finishes for the site landscaping have been carefully considered to deliver a combination of:

- Robustness and fitness for purpose.
- Quality in respect of visual effect and function.
- Definition of place such as key public spaces (local centre), home zones and recreational destinations.
- Legibility in terms of changes of function of space, contribution towards traffic calming and guidance for the visually impaired.
- Cost effectiveness.
- Visual cohesion which is achieved by using a limited but effective palette of materials.

Planting proposals have generally been formulated to complement the local landscape setting. Native trees have been selected in significant numbers for planting along boundaries and across open spaces, while a mixture of native and non-native species have been chosen to be compatible with available spaces such as along roads or in close proximity to houses.

Shrub and groundcover planting provides additional sense of seasonality, diversity and visual interest and includes pollinator species to enhance site biodiversity.

A Landscape and Visual Impact Assessment (LVIA) has been prepared for the EIAR. Based on the assessment of the landscape characteristics, values and sensitivities, a number of viewpoints were selected for assessment of visual amenity impact. These are divided into two categories, namely *Immediate Views (Micro Landscape)* and *More Distant Views (Macro Landscape)*.

The LVIA concludes that there will be a predicted high and neutral significance of impact on landscape character as a result of the proposed development. This conclusion acknowledges the significant change to the landscape character which will arise from the development whilst also taking account of the emphasis placed in the design proposals on the delivery of quality residential environments and sustainable long-term landscape infrastructure across the site that will provide for integration of the development in the sensitive hillside setting.

In terms of visual impact, each of the 20 viewpoints examined generates 6 visual impact assessments relating to the 6 phases of development proposed. This results in a total of 120 assessments. It is anticipated that:

1. The short-term impact will be
  - Low neutral for six of the assessments, Medium neutral for eighteen of the assessments, high neutral for eight of the assessments, no change for seventy-one of the assessments and high adverse for the remaining seventeen assessments.
2. The medium to long term impact will be (i.e. after completion and development of any mitigating landscape treatment that may be required):
  - Medium neutral for twenty-three of the assessments, high neutral for twenty-four of the assessments, no change for seventy-one of the assessments and high beneficial for one assessment and medium adverse for the remaining one assessment.

At the macro (wider) landscape level, views of the proposed development site are generally from vantage points on high ground associated with the southern ridge of the Glen River valley, and high ground along the local road network to the north-west of the site. In these views distance has a diminishing effect such that visual impacts are found to be neutral.

At the micro (local) landscape level, topography and tree cover play a substantial role in limiting views of the site but there will be some significant views of the proposed development arising in respect of views 1, 2 and 5. While short term visual impacts in respect of these viewpoints are found to be adverse, reflecting a level of perceived intrusion in the landscape where the rear elevations of houses are visible, these impacts will be mitigated in the medium to long term as the mitigation measures in the form of tree, hedgerow and woodland screen planting take effect.

The resulting medium to long term impacts will, therefore, be categorised as neutral. The relatively small number of adverse impacts reveals the capacity of the site to accommodate the proposed development while the absence of long-term adverse impacts (with the exception of view 6) reveals the effectiveness of mitigation measures in the form of the emerging green network throughout the subject site. It should be noted that the aim of such mitigation is not to fully screen or hide the development but to provide a sustainable long-term natural framework which will suitably anchor the development in the local landscape setting.

## 7.8 Childcare and Education

A Childcare Provision Assessment has been carried out for the proposed development and accompanies this planning application. This assessment examines the need to provide an on-site crèche facility as part of the development proposal. The assessment involve an audit of existing known childcare facilities in the area. We have also examined the most recent Small Area Profile Statistics available for the area from the 2016 Census. The review has shown that there is some available capacity in childcare facilities in the area. . It also shows that there is a high level of people in the area that principally look after the home / family. It is likely that many of these people also childmind.

Notwithstanding this, a crèche is being proposed as part of the development to meet the additional need arising from the proposed development. The crèche will be 692.88 sq.m and capable of accommodating approximately 103 no. children to meet the needs of the proposed development. Having reviewed the proposed crèche plan, Cork City Childcare Committee confirmed that the proposed service will meet the projected childcare needs in this area and surrounding areas. Please see letter from Cork City Childcare attached at Appendix A of the Childcare Provision Assessment Report.

With regard to education, a School Demand Report has been prepared and accompanies this planning application. . This assessment has been carried out to examine the likely demand for school facilities in the area, having regard to the planning policy context, the existing schools in the area and the estimated number of school places that the proposed development would be likely to generate. The report concludes that there are a number of existing schools in the area and there has been a general decline in enrolment figures over the last number of years. Therefore, existing schools should be capable of providing a number of school places for this first phased tranche of residential development to be proposed in the

Ballyvolane UEA. Given the sites proximity to the City Centre, it is likely that existing and future children in the Ballyvolane area will avail of places in popular City Centre schools.

It is noted that there are two sites zoned for schools within the expansion area. The proposed school campus site is within the ownership of Longview Estates Ltd and adjacent to the proposed development. This planning application demonstrates that the proposed development does not interfere with the provision of a school campus at this location. Further we note that engagement has occurred with Mr Alan Hanlon of the Site Acquisitions and Property Management for the Department of Education and Skills. The Department have confirmed that they have no current plans for the acquisition and development of the relevant zoned school site. Please see copy of correspondence received from the Department of Education and Skills at Appendix A of the School Demand Report.

## 7.9 Services Infrastructure

### *Water Supply*

It is proposed to connect the sites water supply to an existing pipeline present in the Dublin Hill area approximately 780m to the west of the site boundary. The extension of this watermain has been agreed with Irish Water and will comprise a 250mm HDPE watermain.

### *Foul Water*

The topography of the site generally falls from the east towards the Ballyhooly Road to the west. This allows for the majority of the network to be gravity fed with the exception of phase 5 on the far eastern side of the site. It is indicated below how the foul network will develop as the various phases are complete.

Phase 1: Foul network will be gravity fed and will connect to existing 225mm foul sewer running north to south on Ballyhooly Road.

Phase 2: A new strategic pump station is required along Ballyhooly Road to the south of the residential development. This station is required to accommodate additional phases and future developments in the Urban Expansion Area (UEA). The existing foul network has capacity for Phase 1 only. The applicant has entered into a Project Works Service Agreement (PWSA) with Irish Water for the delivery of this infrastructure.

Phase 3: Additional foul network required for Phase 3 housing will be tied into development foul network and be gravity fed to new Irish Water pumping station.

Phase 4: Additional foul network required for Phase 4 housing will be tied into development foul network installed along Ballyhooly Road and be gravity fed to new Irish Water pumping station.

Phase 5: Due to topography constraints, wastewater from Phase 5 will need to be pumped in order to connect to the overall development foul network. A new pumping station will be constructed bordering Phase 5 to achieve this. The rising main from the pumping station will extend north along the main distributor road through the proposed development before tying into the overall development foul network at a location adjacent to Phase 2. Wastewater will then be gravity fed to the new Irish Water pumping station.

Phase 6: Additional foul network required for Phase 6 will be tied into development foul network and be gravity fed to new Irish Water pumping station.

### *Surface Water*

A combination of infiltration to the east and stormwater attenuation to the west of the site is proposed to drain the development. Soil infiltration rates to the east of the site were identified as being high while infiltration rates to the west of the site were low. These results informed the design team that a Sustainable Urban Drainage System (SUDs) compliant system could be used for surface water collection for the eastern portion of the proposed development while the remainder of the site would need to be positively drained off the site via attenuation tanks. In areas where soil infiltration was not possible due to topography and soil type, SuDS type measures such as the use of permeable paving on internal junctions and open drainage swales are being proposed.

To ensure a robust design, attenuation flow rates were restricted to  $Q_{bar}$  rates for each of the individual phases. The use of soakaways for surface water infiltration is proposed in locations generally to the east of the site (in phases 2 & 5). Surface water quality will be treated through the use of Oil Separators and SUDS measures. For this development, the following SUDS measures are proposed:

- Planted swales running adjacent to roadways where feasible.
- Kilsaran permeable paving at suitable locations throughout the site.
- Storm-tech attenuation chambers in conjunction with Hydroflow vortex control to maintain a maximum outflow of 5 l/s/ha (Avg  $Q_{bar}$ ).
- Infiltration soakaways on the eastern portion of the development where the topography is flatter and infiltration tests were conducive to infiltration.

It is proposed to connect the main surface water discharge to the local network at a location 0.8km south on Ballyhooly Road, Outfall 2. Neighbourhood 4 (Phase 4) will connect at Outfall 1, and will be used to recharge the existing stream during low flow periods.

The proposed tie-in locations were selected following discussions with Cork City Council. The proposed outlets into the existing watercourse will incorporate outfall header walls to mitigate riverbed (stream) erosion; no works will occur within the stream.

### 7.10 Archaeology

Chapter 11 of the EIAR provides an assessment of the impacts of the proposed development on the recorded and potential cultural heritage resource and identifies appropriate mitigation measures. The assessment found that there are no recorded archaeological sites located within the proposed development site, or within 120m of its boundary, while there are five examples located within 300m of the site boundary. These comprise a levelled ringfort (CO063-114----) and the recorded site of an underground souterrain (CO063-115----) in lands to the west and three levelled burnt spreads in the lands to the south (CO074-131----, CO074-132---- and CO074-172--). None of these sites are visible from the proposed development site. No potential archaeological sites were noted within the proposed development site during the desktop study and site inspections.

The Record of Protected Structures and the National Inventory of Architectural Heritage (NIAH) do not list any structures located within 600m of the proposed development site and there are no buildings of any date located within the site boundary. The earthen field bank which forms the boundary between Lahardane and Ballincolly townlands extends through an area in the south end of the proposed development site and is deemed to be of low cultural heritage significance.

Given the scale and extent of the proposed development within an undeveloped area of farmland, a programme of pre-development licensed archaeological investigations, to comprise a geophysical survey followed by archaeological test trenching, will be undertaken across the proposed development site. It is noted that the Cork City Council report to An Bord Pleanála in relation to the proposed development stated that these mitigation measures were acceptable.

The assessment concludes that the proposed mitigation measures will provide for either the avoidance of the cultural heritage resource or the proper and adequate recording of this resource (including currently unknown archaeological features). As a result, there will be no predicted residual impacts on the cultural heritage resource following the construction phase.

## 7.11 Ecology

Chapter 10 of the EIAR assesses the likely significant effects on biodiversity (flora and fauna) arising out of the proposed development. Measures to mitigate the potential impacts on defined ecological features are proposed. The assessment involved a desk study and field surveys by a suitably qualified ecologist. The key findings from the assessment include the following:

- No features of significance for roosting bats are present within the proposed site, however bats do commute to the site to forage.
- No evidence of badger setts was recorded within the proposed site. Badger droppings were present to the east of the site, indicating that this area is part of a badger territory.
- The treelines and more substantial areas of hedgerow provide habitat for breeding birds. Three species of moderate conservation concern and one species of high conservation concern were recorded at the site.
- Badger, bats and birds are of Local Importance (Higher Value) as they occur at the proposed site and are considered to be ecological features. The habitats at the proposed site are not suitable to support other protected species of fauna.
- No impacts will occur to roosting bats. There is potential for the loss of hedgerows and treelines and use of lighting to impact on commuting and foraging bats. In the absence of mitigation, this would be a significant impact at the local level.
- As no badger setts were recorded at the site, as such, there will be no direct impacts to badgers. However, badgers create new setts regularly, and the site provides suitable habitat for sett excavation in earth banks associated with hedgerows/ treelines at the site. Direct impacts on badgers may occur should badgers establish setts in hedge banks adjacent to areas of construction. This would be a significant impact at the local level.
- The proposed development may reduce the foraging area of badgers, however, in view of the small area of the site utilised by badgers and the abundance of available habitat in the wider landscape, this is not expected to have a significant impact on the local badger population.



- The loss of hedgerows, tree lines and arable fields may impact on nesting birds and will result in a reduction in foraging area for yellowhammer. In the absence of mitigation, this would be a significant impact at the local level.

In order to compensate for the loss of hedgerows and treelines, significant new planting has been incorporated into the landscape design for the proposed development. This planting predominantly comprises native species, with some non-native species included as street trees and specimen trees. The landscape design ensures that habitat connectivity in the wider area is maintained and includes planting of tree species that are of value to pollinators. The open space in the wayleave also includes a wildflower meadow, which incorporates several ornamental species of value to pollinators.

The landscaping plans mitigate the loss of foraging and commuting habitat for bats. To minimise disturbance to bats and other fauna that are active at night, construction lighting will be directed away from all hedgerows and treelines to be retained. The proposed landscaping plan and the lighting plan provide darker areas within the site and potential foraging areas for bats. A pre-construction survey shall be undertaken prior to the commencement of construction to identify active badger setts occurring within the site. In the event that badger setts are identified within proximity to the proposed works area, the mitigation measures provided in NRA (2005) will be fully implemented.

No hedgerow clearance or tree felling will occur during the bird breeding season from 1st March to 31st August. The landscaping proposals will mitigate for the loss of habitat utilised by birds for foraging and shelter. The landscaping plan also includes for the installation of bird boxes in the wayleave area. The proposed development will result in a permanent reduction in the available foraging area for yellowhammer. However, in view of the extensive arable and pasture farmland habitat available in the surrounding landscape and the landscaping incorporated into the project design, the residual impact on the local yellowhammer population is not expected to be significant. Overall, no significant residual impact on biodiversity are expected as a result of the proposed development.

#### 7.12 Natura Impact Assessment

A Natura Impact Statement (NIS) has been prepared Greenleaf Ecology and is included as a standalone report with the planning application. The NIS concluded that, with the implementation of best practice and the recommended mitigation measures, there will be no potential for direct, indirect or cumulative impacts arising from the proposed residential development, either alone or in combination with any other plans or projects. No reasonable scientific doubt remains as to the absence of such adverse effects.

#### 7.13 Environmental Impact Assessment (EIAR)

Annex I of the EIA Directive 85/337/EC requires as mandatory the preparation of an EIA for all development projects listed therein.

Schedule 5 (Part 1) of the Planning & Development Regulations 2001 (as amended) transposes Annex 1 of the EIA Directive directly into Irish land use planning legislation. The Directive prescribes mandatory thresholds in respect to Annex 1 projects.

Schedule 5 of the Planning and Development Regulations 2001 (as amended) sets out a comprehensive list of project types and development thresholds that require a

mandatory Environmental Impact Assessment. The proposed development falls within classes of development set out in Part 2 of Schedule 5. The applicable categories are as follows:

*Infrastructure Projects*

*10. (b)*

*(i) Construction of more than 500 dwellings and*

*(iv) Urban development which would involve an area greater than 2 hectares in the case of a business district, 10 hectares in the case of other parts of a built-up area and 20 hectares elsewhere.*

The proposed development triggers a requirement for an EIA because the number of dwellings proposed is 753 which exceeds the threshold of 500 specified in 10b (i) above and the overall site area is 46.9Ha which is in excess of the 20 hectares threshold for urban development elsewhere in 10b (iv) above.

The EIAR included with this planning application has been prepared by a specialist and competent team of experts.

#### 7.14 Part V

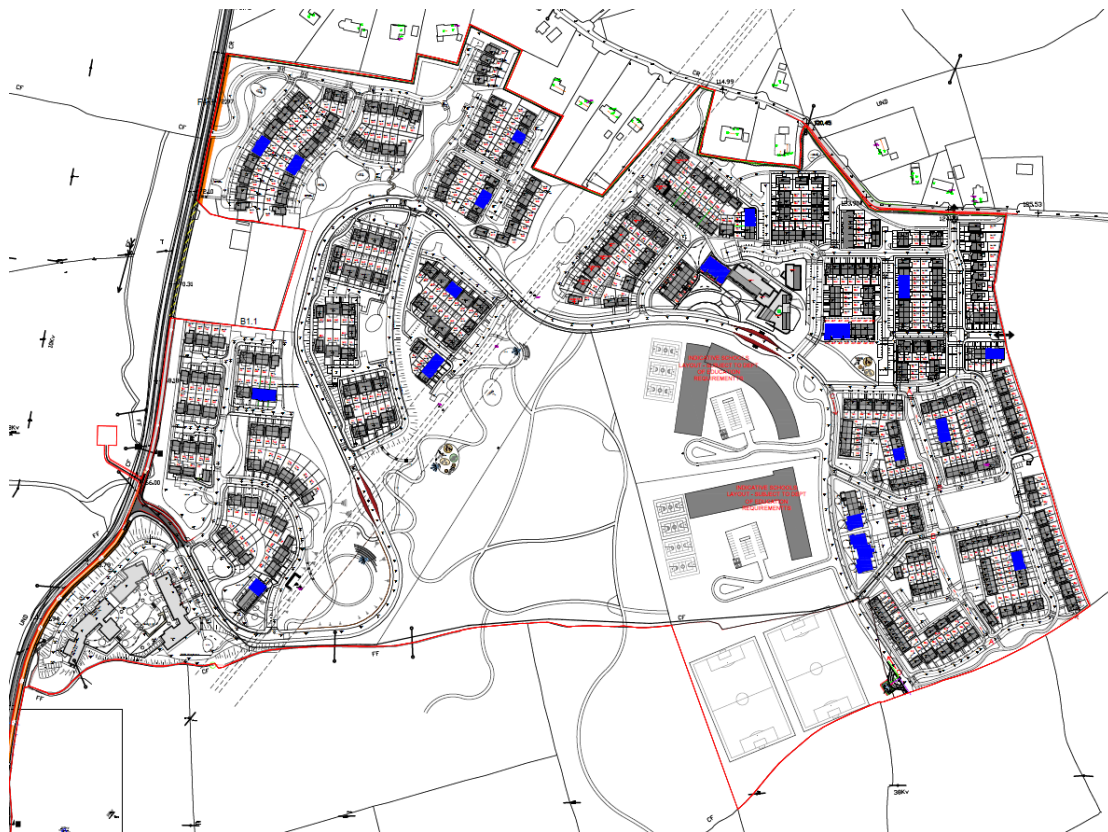
Pursuant to Section 96 of the Planning and Development Act 2000 (as amended), 10% of the units are proposed for social housing. Engagement has occurred with Mr Brian Geaney of Cork City Council Housing Department regarding the Part V proposal and these units have been agreed. Please see attached records at Appendix C.

Longview Estates Ltd propose to meet the site specific part V obligation through the transfer of 75 no. units as follows:

- 17no. 2-bed ground floor duplex apartments
- 19 no. 3-bed duplex units
- 11 no. 3-bed end of terrace units
- 8 no. 3-bed mid terrace units
- 17 no. 2-bed mid terrace units
- 2 no. 3-bed semi detached units
- 1 no. 4-bed semi detached unit

These units have been dispersed throughout the site as can be seen on the enclosed Horgan Carroll drawing. The proposed Part V calculations are included at Appendix D.

**Figure 14** Location of Part V Units



## 8.0 Conclusion

The proposal provides for a highly sustainable community on lands zoned for residential development. The scheme provides for 753 no. residential units which will contribute to the realisation of housing targets for Cork as set out in the National Planning Framework and the Regional Spatial and Economic Strategy for the Southern Region. The proposed scheme has been designed to a very high quality in accordance with the relevant design policies and guidance and proposes a broad range of unit types and sizes, a local centre and crèche to meet the needs of people at various stages of the life cycle and to ensure a mixed tenure. The scheme includes attractive and well considered open spaces and a 3h hectare park which incorporate walking and cycling routes throughout the site for the future occupants.

The density proposed has been informed by the need to encourage higher densities on outer suburban, greenfield sites and the site specific constraints. The density of 35.7 units per hectare is considered appropriate particularly in light of the sites rising topography and ground conditions.

The accompanying Statement of Consistency demonstrates how the proposed development is consistent with the relevant national, regional and local planning policies.

The proposed development will result in the creation of a new residential community at a sustainable location which has been identified as a key expansion area for the city. The proposed development will make a significant contribution to housing in Cork in the short to term medium term to address the acute housing crisis.

**Appendix A Growing the Opportunities for Cork City's Northside – Cork Chamber Chamberlink Magazine Issue 4 2018 and letters from Cork Chamber of Commerce and Cork Business Association**

COVER STORY

## GROWING OPPORTUNITIES ON THE NORTHSIDE

*Cork Chamber CEO Conor Healy discusses the opportunities for the Northside of Cork City in light of Project Ireland 2040.*



Conor highlights the need for much improved and diversified transport infrastructure, the employment potential and the overall latent opportunities for our City.

Cork is growing at pace, with an estimated 25% of Cork City's footprint to be built in the next 25 years. We need to be smart and strategic in how we grow, to learn from past experiences, to be visionary and proactive.

It is crucial that as a City region we build on the opportunities for Cork and in doing so, we must recognise the existing depth of potential in our City. On our doorstep we have vast opportunities in the development of the City Dockland, coupled with the enormous latent potential and opportunities for our existing urban areas and strategic employment areas.

For instance, with investment the northside of Cork City could reflect perfectly the ambition of Project Ireland 2040 for compact and sustainable growth. Project Ireland 2040 aims to provide balanced

regional development and to improve the State's infrastructure. It plans for an additional one million people living in the state in the next 25 years and places its emphasis on the use of brownfield sites in cities, towns and villages. It is estimated that Cork will take 20% of this population growth. To do this, we need to proactively reengineer our City's districts and communities to grow sustainably to meet future population demands while maintaining and growing the quality of life, uniqueness and heritage of our City region.

Right here and now, we have a unique and diverse offering in Cork underpinned by an impressive business, skills, education, international connectivity and cultural track record. We have seen the City population increase by 4.6% in recent years, and we have seen a significant increase in business activity with Cork going from strength to strength as an attractive location for FDI while being the base for a growing number of hugely successful home-grown companies. We have strong and established clusters in ICT, pharma, life-sciences, cyber security, energy, marine, agri-food and financial services.

Crucially though, to ensure that we grow our potential and future proof our City region, we need to focus on the foundations. One of which is public transport. We need a network and infrastructure that works with Cork and can grow and adapt as the City changes and grows. This is fundamental if we fail to plan for the future, the alternative is unmanageable congestion and the lowering of our quality of life with negative knock-on effects on society, the environment and the economy.

If we stick with the northside as an example, there is already a hugely impressive portfolio of successful companies with names such as Teamwork.com, Xanadu, Blizard, Westbourne IT, Senecom, Quintus, Flew and of course Apple. There is the Northside for Business Campus focused on developing enterprises and growing small businesses, and there is a whole host of home grown indigenous businesses. These companies, and others, have recognised the potential here, harnessing their operations a few short kilometres from the City Centre.

This year, we've had positive announcements such as the M20 Cork to Limerick motorway bringing with it the development of the eastern section of the Northern Ring Road. These projects will be massively influential and transformative. Of course, the development of the complete Northern Ring Road route will catalyse this potential even further.

However, we can't afford to overlook the opportunities to develop the public transport infrastructure across the northside of the City, by increasing the accessibility and permeability as a City region we are investing in the community both business and residential. For instance, the train runs straight through from Malton to Kent Station. It is only logical and a quick win to put that back into motion to develop the Wilbury Train Station servicing Blackpool and the surrounding areas. If we are to grow sustainably and ensure a public transport system that meets the needs of the City, it is imperative that such access points are developed without delay. We need safe cycle

**It is only logical and a quick win to put plans back into motion to develop the Wilbury Train Station servicing Blackpool and the surrounding areas.**

path infrastructure in and around the northside, and the orientation of the public bike share scheme to service the area.

Previously there were proposals for a Bus Rapid Transit (BRT) system to run from North to South connecting Rathpolina to Cork Airport via the City Centre. Wouldn't it be timely to now re-examine the potential of this North to South BRT route, linking with the key strategic employment areas at Blackpool and Cork Airport. This would greatly increase the attractiveness and appeal of bus transport as an option for commuters, and also be the preferred alternative with the East to West, Ballinacorney to Malton BRT.

The dedicated BRT corridor could be linked to Park and Ride on the northside of the City, again this would facilitate options for commuters freeing up congestion in the City and giving people a plausible

alternative option to private car. Why not have the Wilbury Train Station developed to operate dually as the Park and Ride location connected to a dedicated bus corridor and with a public bike share station and secure private bike parking also located at the station and with the appropriate connecting infrastructure in place? The time is now to start developing the game-changers. At a later stage these bus corridors could be transformed into a light rail Corridor.

Ultimately a new type of urban living needs to be developed with higher density corridors and better public transport serving commuters and residents alike. City regions must be enabled to grow their unique opportunities. As part of Project Ireland 2040, the Government has committed €200m to the Connects Programme funding to Cork. We need this made available without delay to facilitate the proactive development of an enhanced public transport network, with the commitment to future public transport investment. The northside of Cork City is on the cusp

**As a City region we are investing in the community both business and residential.**

As a nation, we need to transform into a smarter, more urban and climate friendly country, and here in Cork, we have the opportunity to be a leader in this space, to realise the vision of Ireland 2040.





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John Crean  
Longview Estates Ltd  
Unit 74  
Penrose Wharf  
Penrose Quay  
Cork

15 April 2019

**Re: Proposal for 753 Housing Development by Longview Estates Ltd SHD in Ballyvolane / Laherdane, Co. Cork**

Dear John,

Cork Chamber of Commerce welcomes the proposed development of 753 residential homes in Laherdane/Ballyvolane by Longview Estates Ltd.

As the leading business organisation in the Southern Region, Cork Chamber comprises 1,200 members who employ over 100,000 people in the region. Our mission is to make Cork the best place for business.

The development of more housing across Cork is of critical importance to the economic future of this region. Under Project Ireland, national growth targets will see a doubling of Metropolitan Cork's population to over 500,000 people, including an additional 75,000 people in Cork city and suburbs by 2031. In order to meet these targets, Cork city and suburbs will require a minimum of 2,500 additional housing units every year to 2040.

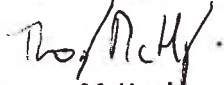
This proposal by Longview Estates Ltd not only provides 753 housing units, but also much-needed services infrastructure that will open Ballyvolane to further development and investment. As such it represents a major investment in an area that historically hasn't been a focus for public or private sector investment. The development also builds on Cork City Council's recent public transport initiatives on Ballyhooley Road and it is strategically located close to Blackpool – a key employment area in Cork City which has seen significant growth in recent years, particularly in the IT sector.

The importance of the 'Northside' in the growth of Cork has been recognised by the Chamber of Commerce. In our recent Chamberlink publication (Q4 2018), we highlighted the opportunities open to the Northside of Cork City to deliver on Project Ireland 2040. In the same way that Cork's Docklands is of strategic importance to Cork's future, so too is investment in housing and residential spaces on Cork's northside.

This investment creates an impetus for public transport investment, rebalancing of the City, and alleviate pressures on infrastructure and services in the southern suburbs. In addition, this

development will encourage investment in the northside and lead the way in encouraging further focus on residual lands in the Ballyvolane area and Monard further north west of the City. Consequently, we commend the proposal to An Bord Pleanála.

Yours Sincerely,



**Thomas McHugh**  
Director of Public Affairs



**CBA**  
Cork Business  
Association

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John Crean  
Longview Estates Ltd  
Unit 74  
Penrose Wharf  
Penrose Quay  
Cork

8<sup>th</sup> April 2019

**By Post / Email**

**Re: Longview Estates Ltd SHD Proposal Ballyvolane / Laherdane, Co. Cork  
Proposal for 753 Housing Development**

Cork Business Association welcomes the proposed advancement of this residential development housing in Laherdane by Longview Estates Ltd. The proposal will not only provide housing but will also provide much needed services infrastructure that will open up the area to further development and investment. As such it represents a major investment in an area that historically has not been a focus for public or private sector investment and is only now seeing fresh investment from the City Council through public transport initiatives on Ballyhooly Road.

Cork is experiencing significant growth in employment space without the necessary provision of residential opportunities for its new and expanding population. The Ballyvolane Urban Expansion area has been identified by the RSES as a location for needed development. The Local Area Plan and County Development Plan also identify the area as a location for growth. This proposal delivers this growth impetus.







**CBA**  
Cork Business  
Association

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In the north of the City, this investment will support Public Transport investment, rebalancing of the City which has historically seen development and expansion of the more affluent areas south of the Lee, and alleviate pressures on infrastructure and services in the southern suburbs.

This development will encourage investment in the northside and lead the way in encouraging further focus on residual lands in the Ballyvolane area and Monard further north west of the City and we welcome the initiative.

Yours sincerely,

**Chief Executive**



**Appendix B** Copy of Public Consultation Presentation Boards



# Longview



- Community
- Buffer/Shelter Boundary & Embankment
- Embankment/Cut
- Open Space/Soft
- Site Planting
- Playground



# Longview

## History & Location



**The proposed site for Longview is located less than two miles from Cork City centre. Its name literally means Halfway Hill, or more conversationally, The Gentle Slope.**

It lies in the townland of Lahardane, within the Ballyvolane Urban Expansion Area of the approved Local Area Plan.

Much of the land in the area was once owned by the Coppinger family, originally from Denmark and who arrived in Cork in the 1600s.

Alongside this development runs the Ballyhooley Road, the name of which comes from Baile Atha na hÚbhla, or Town of the Apple Ford, from when the medieval town of Ballyhooley was famous for its apples.

What is most striking about this townland, is the panoramic view of Cork from the city to the hills, almost all the way to Ballyhooley itself.



It is the combination of this view, its unique proximity to the city and its potential to create a vibrant new community which makes this picturesque and undeveloped pocket of a growing Cork, so attractive.

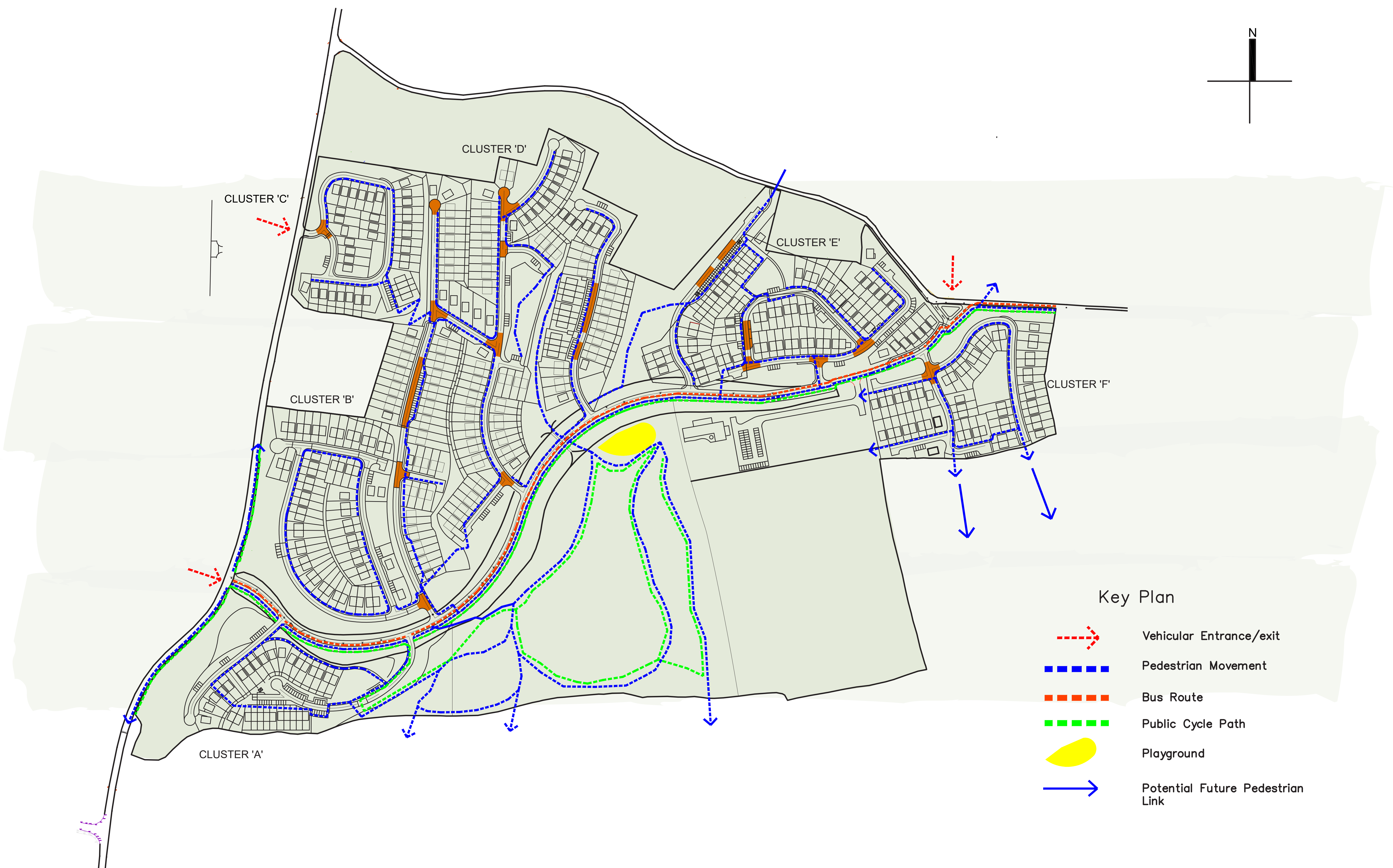
**“It is a rare & fantastic opportunity to integrate this benchmark development with such great views of the county beyond, while being so close to Cork City centre.”**





# Longview

## Connecting Communities



**Permeability, (ease of access between clusters and the park), for residents of Longview, along with external connectivity to the city are central facets of the design of this development.**

Extensive scenic cycle paths and landscaped pedestrian walkways will be provided throughout. The design incorporates a proposed footbridge crossing the central spine road, facilitating safe, unrestricted access to the

parkland and the avoiding the need to cross the road, (additional pedestrian crossings will also be provided).

Furthermore, bus routes will be extended through Longview to ensure ease of access to the city centre while also servicing the proposed schools in the area. Bus routes will operate through the three strategic transport corridors in the immediate area, those being Blackpool, Ballyvolane, and Mayfield.

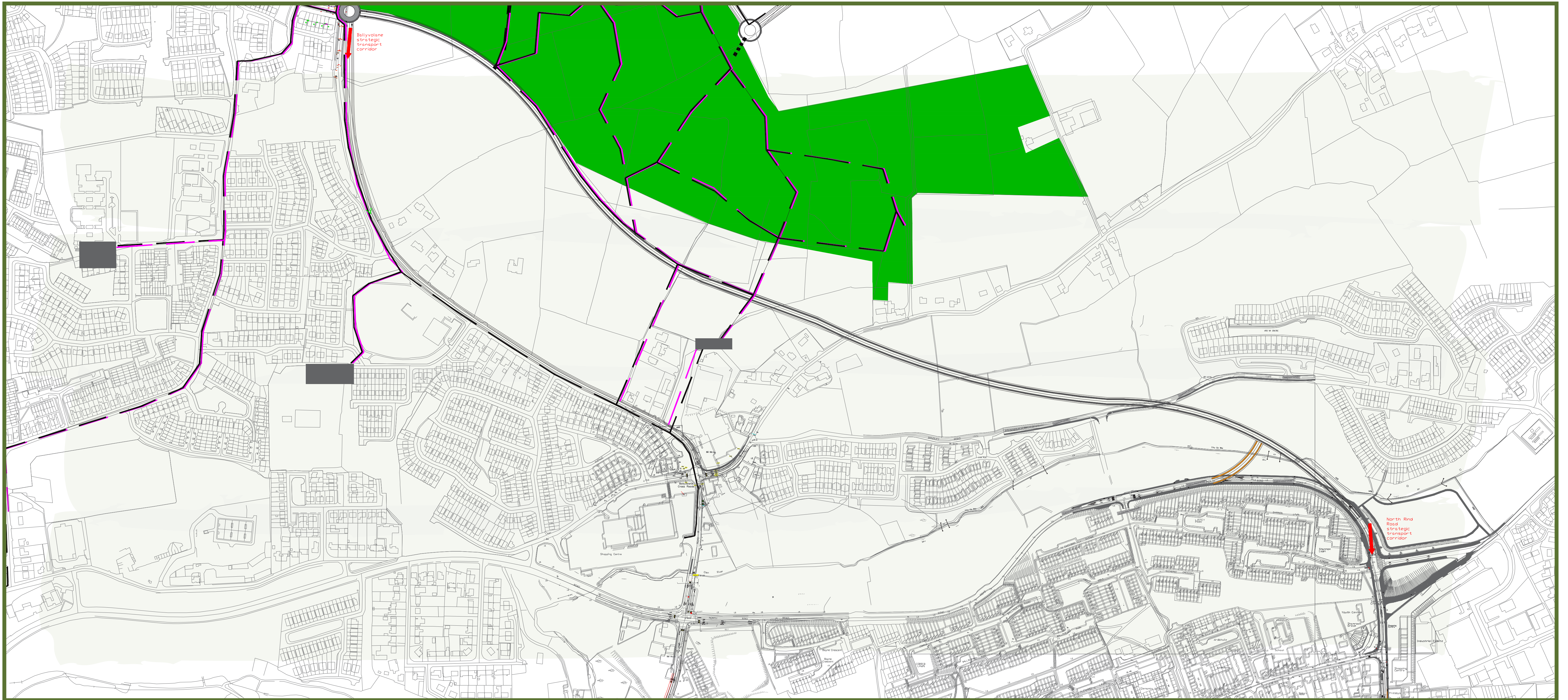
**“A dedicated bus lane with frequent services will provide residents with access to employment locations in the City Centre, the South West and South East of the City. This will also enhance the connectivity to the proposed schools...”**





# Longview

## Traffic & Drainage



### Traffic:

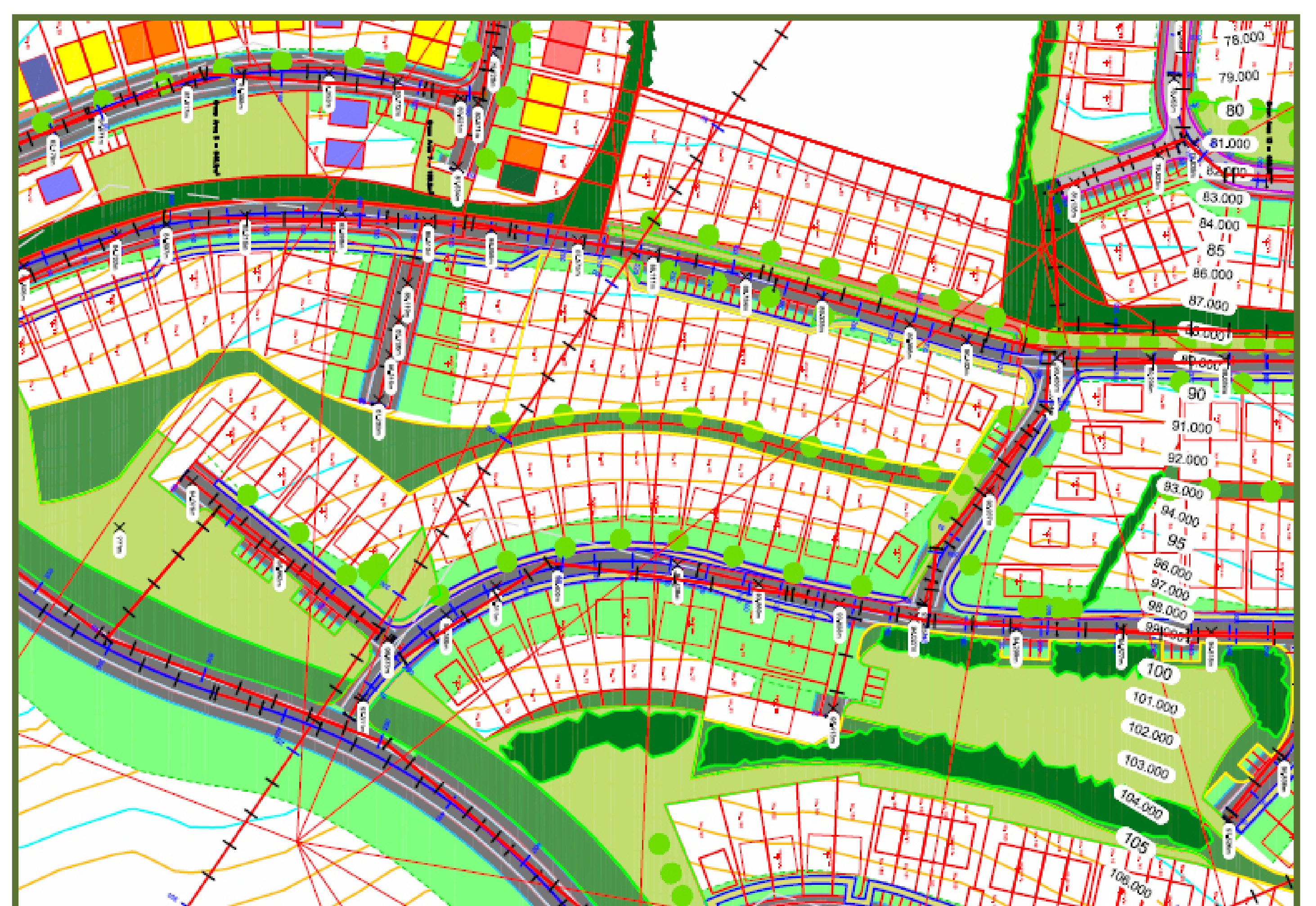
**An unusually detailed level of traffic modelling was undertaken to address concerns surrounding future capacity.**

Seven separate junctions within the surrounding area were analysed, and forecasts of future potential traffic volumes every year between 2017 and 2024 were estimated.

Significant interaction with both the Cork City and County Councils has been progressed and proposals to address potential issues at these junctions have been agreed.

The surrounding area will benefit from the development of a number of link roads which will run from Mayfield to the North Ring Road, (part of LAP), which will offer residents a number of routes and transport options. Increased bus routes and pedestrian/cycle facilities have been incorporated, offering alternatives to car usage.

The trunk road connection to the north, (at Cluster F), will not be completed until the final phase, and after the Mayfield link road has been completed, hence there will be no additional traffic on this minor route.



### Drainage:

Longview Estates Ltd. understands that lower lying areas in the city are susceptible to flooding. A detailed Drainage Report has been compiled, with significant positive outcomes.

The current greenfield run-offs have been quantified, and the surface water drainage strategy for Longview has been designed so as to actually reduce water run-off from their current levels. This will have an important affirmative impact downstream, particularly for Blackpool, which can be especially prone to flooding



# Longview

## House Design & Sample Images



**A range of 20 home types have been designed for Longview that will create variety and diversity throughout the clusters.**

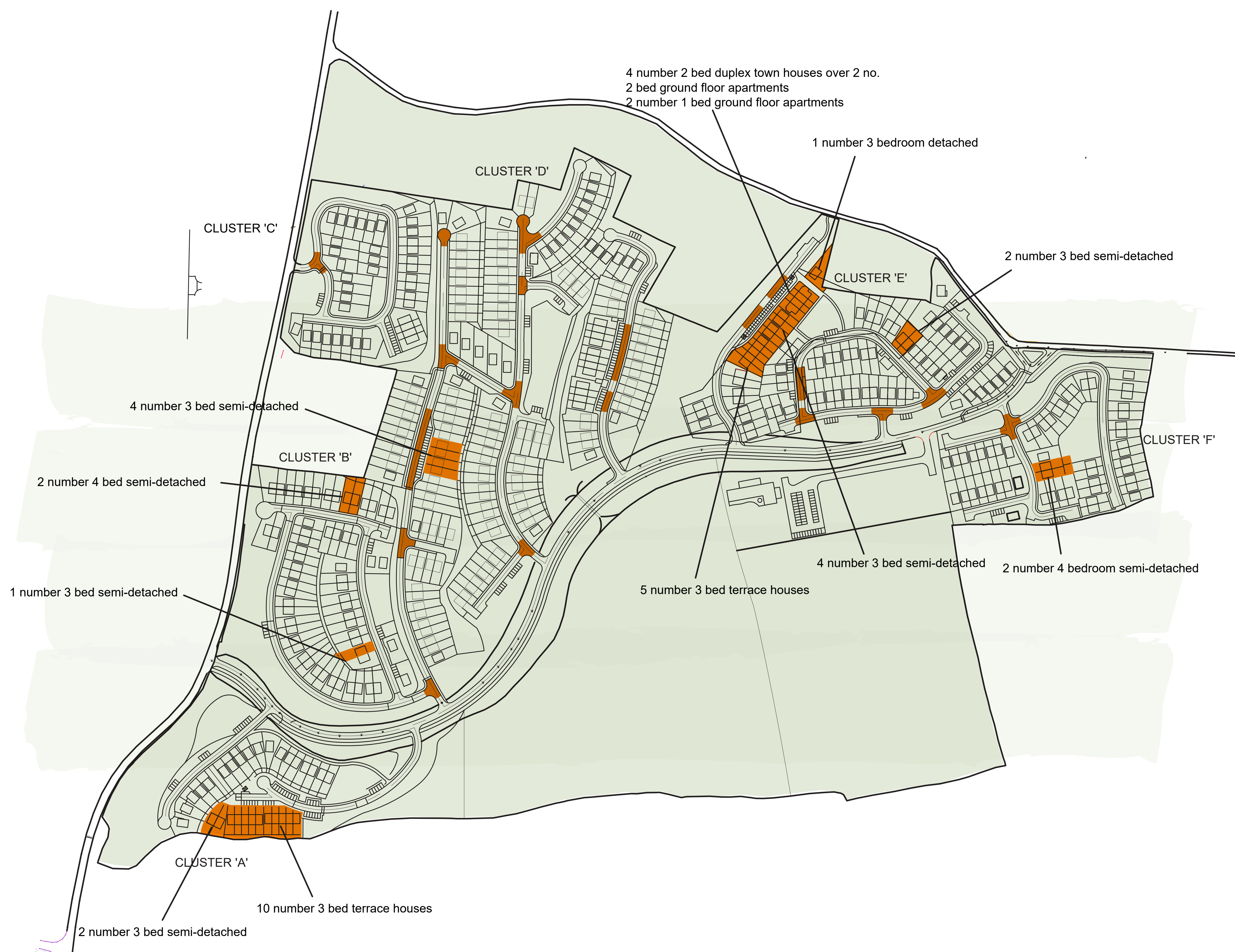
High-quality, long-lasting materials will be used for construction and finishes, with the intention of creating a benchmark standard for homes in the area. Internally, the proposed 2.7m floor to ceiling provides an opportunity for higher than average windows, generally resulting in additional natural light internally and increased passive solar gain.

Single-storey extensions to the rear of dwellings ensure that glazing is facing in an easterly, southern, or westerly direction.



# Longview

## Social & Environmental



**Longview Estates Ltd. is acutely aware of its social responsibilities, and is strongly committed to supporting current guidelines.**

Consequently the development will provide over 40 social houses in line with Part V policy.

This will include a mix of terrace, semi-detached, & detached homes, pepper pocketed throughout the development. We understand the need to engage with both the Council & councillors to deliver these homes to the highest of standards.

The planting of mature native tree species and preservation of existing hedgerows will be a central aspect of the landscaping, which will enhance appearance and the ecological sustainability in the area.

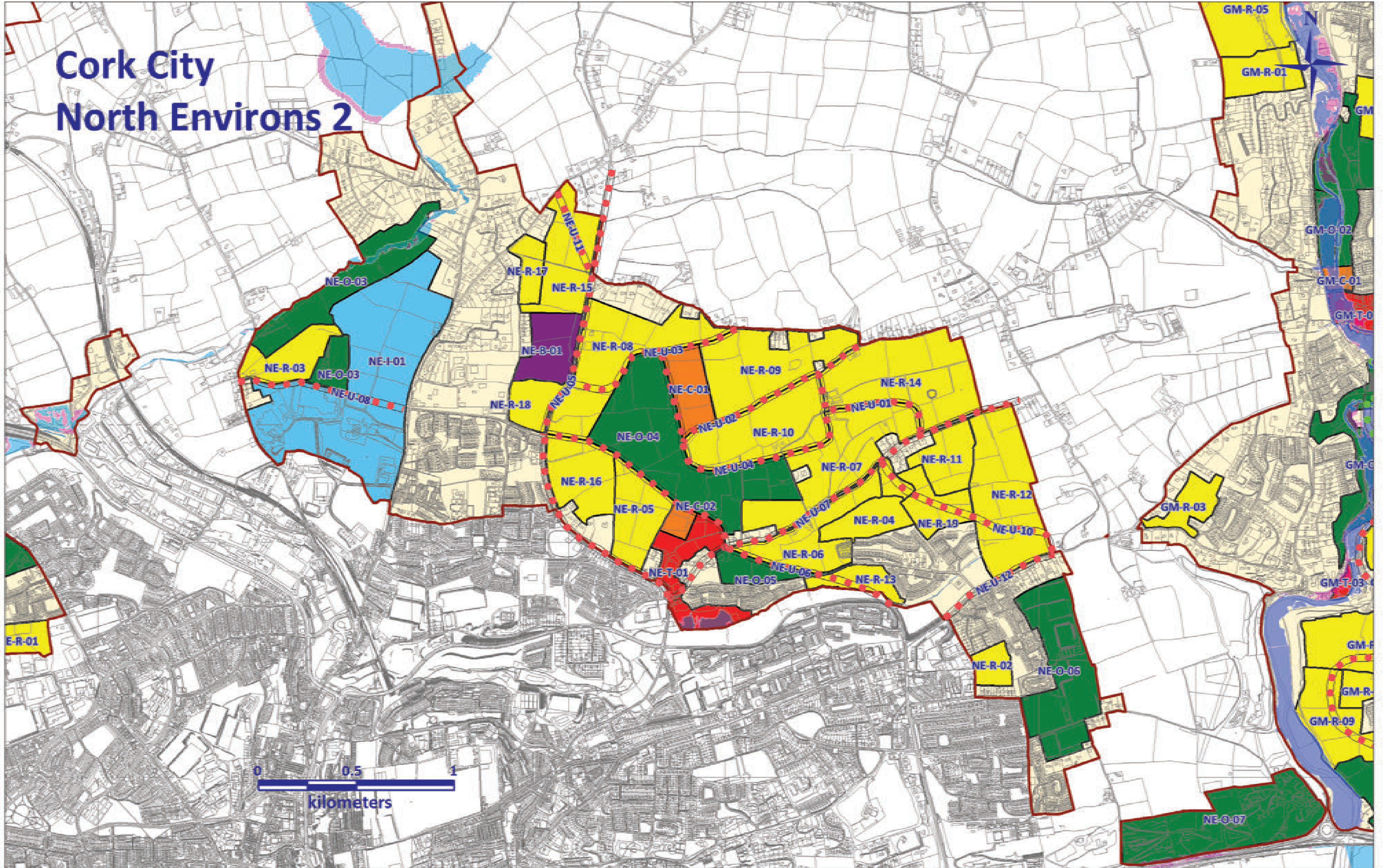
A comprehensive Environmental Impact Assessment, (EIA), has also been undertaken.

**“No rare or protected plants and no invasive non-native plants species were recorded on site. There was no evidence of protected species of amphibians, invertebrates or non-volant mammals at the site...”**





# Cork City North Environs 2



- |                        |   |  |            |                     |           |                                      |                                      |
|------------------------|---|--|------------|---------------------|-----------|--------------------------------------|--------------------------------------|
| Settlement Boundary    | Open Space / Sports<br>Recreation / Amenity | Town Centre /<br>Neighbourhood Centre<br>Community / Utility | Industry   | Special Policy Area | Utilities | Roads                                | Area Susceptible to Flooding: Zone A |
| Existing Built-up Area | Residential                                 | Business   | Enterprise | Retail Core         | Walkways  | Area Susceptible to Flooding: Zone B |                                      |





# Longview



# Longview

## History & Location



**The proposed site for Longview is located less than two miles from Cork City centre. Its name literally means Halfway Hill, or more conversationally, The Gentle Slope.**

It lies in the townland of Lahardane, within the Ballyvolane Urban Expansion Area of the approved Local Area Plan.

Much of the land in the area was once owned by the Coppinger family, originally from Denmark and who arrived in Cork in the 1600s.

Alongside this development runs the Ballyhooley Road, the name of which comes from Baile Atha na hÚbhla, or Town of the Apple Ford, from when the medieval town of Ballyhooley was famous for its apples.

What is most striking about this townland, is the panoramic view of Cork from the city to the hills, almost all the way to Ballyhooley itself.



It is the combination of this view, its unique proximity to the city and its potential to create a vibrant new community which makes this picturesque and undeveloped pocket of a growing Cork, so attractive.

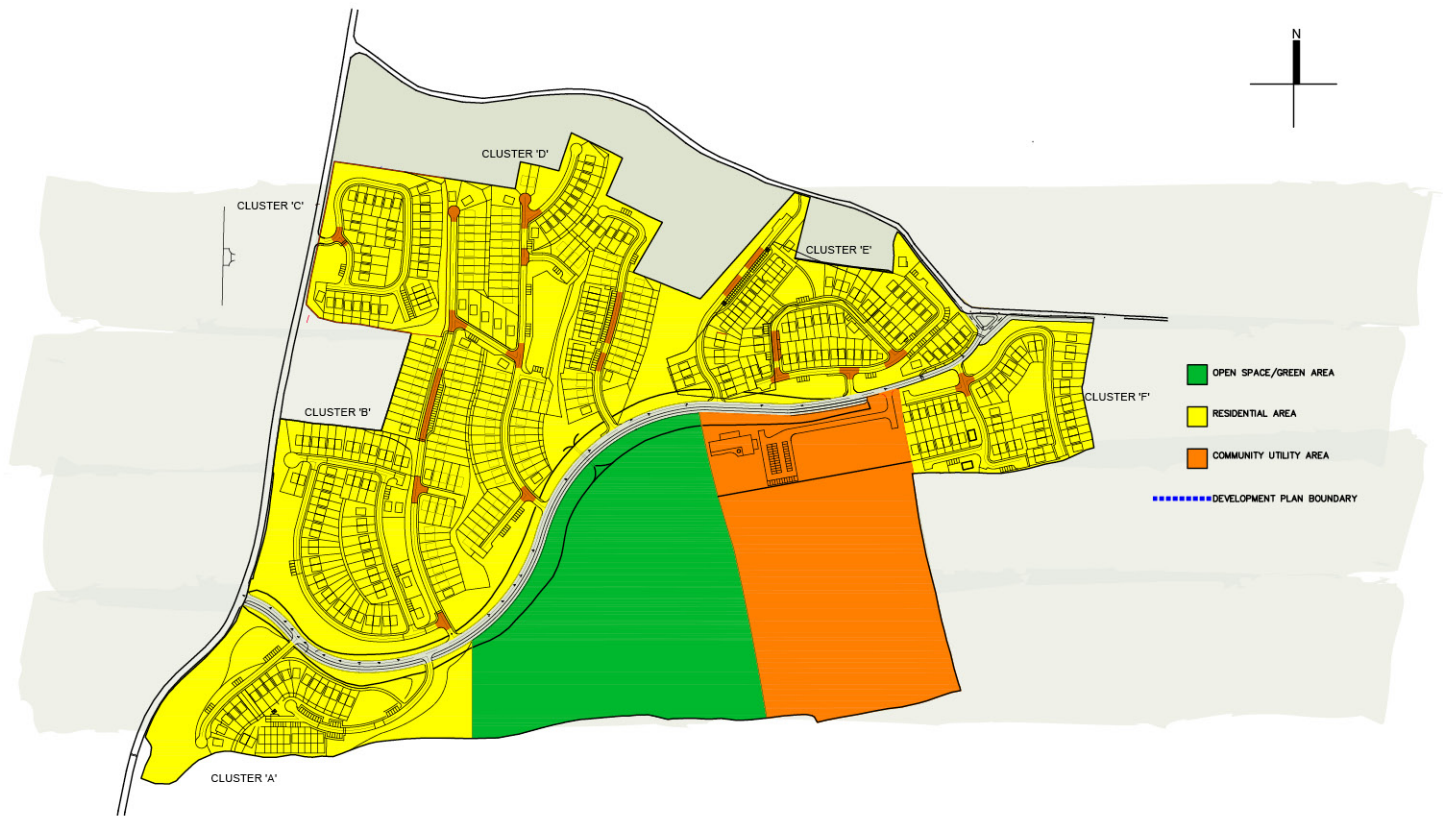
**“It is a rare & fantastic opportunity to integrate this benchmark development with such great views of the county beyond, while being so close to Cork City centre.”**





# Longview

## Zoning Map



**The Council's vision for Cork City North Environs to 2020 is to reinvigorate the northern suburbs of the city, as a significant location for future residential development.**

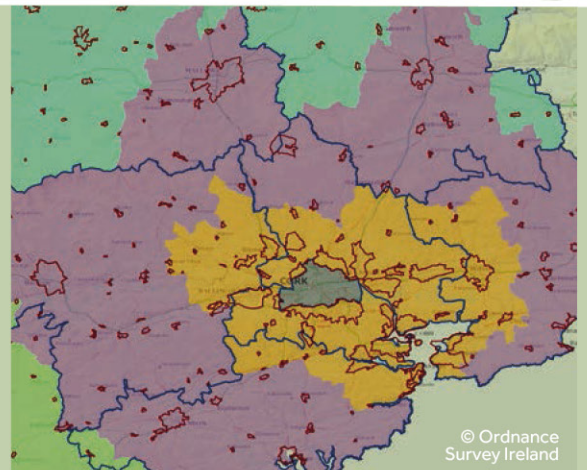
This will require a planned major mixed use development, coordinated with substantial infrastructure investment, the provision of enhanced community, recreational facilities and public transport accessibility, with the aim of rebalancing the provision of services more equitably throughout the city (*Cork Municipal District Local Area Plan—2017*).

What is especially unique about Longview is that residential, community, education, recreation, commercial zoning

and transport corridors all exist as part of the zoning and transport policy for this area. Significant work will be undertaken as part of this development to enhance the local infrastructure.

So, while Longview will ultimately exist as part of a greater neighbourhood, it can be an entirely functional community on its own.

**“Longview is a benchmark development on the north side of Cork City, offering a new standard of quality for residential homes & living.”**



# Longview

## Overall Site Layout



“Longview aims to grow a diverse community, by offering various home types & sizes in self-contained clusters, whilst also allowing for permeability throughout the entire development”

**This development will consist of 451 homes delivered across three phases in six self-contained clusters.**

The housing mix aims to provide a range of homes for single occupants to professionals to larger families including:

- 4&5 Bedroom Detached
- 3&4 Bedroom Semi/Detached
- 2 Bedroom Terrace
- Duplex & 2 Bedroom Apartments



# Longview

## Our Design Goals



**The goal with Longview is to create a benchmark residential development on the north side of Cork City, offering high quality homes, which are integrated into the large zoned green space adjoining the site.**

The design brief necessitates that the concept of sustainability extends from individual house design, which primarily considers occupants comfort and affordability via energy and construction sustainability, be extended to encourage the development of a sustainable community.

Central to this ambition is the development of user-friendly public spaces by means of good design of the proposed

amenity areas. This includes the design of ancillary green areas and pocket parks, in conjunction with play areas within the residential clusters, as well as integration with the adjoining park.

Public transport, pedestrian and bicycle access through the site, and future linkage via adjoining zoned lands to the Ballyvolane neighbourhood and city centre, are perceived as integral to achieving these goals.

The brief also requires that the development retains its high standard of appearance long into the future, and that quality materials consistent with such ambitions are used throughout.

**“It is a rare and fantastic opportunity to integrate this benchmark development with such great views of the county beyond, while being so close to Cork City centre.”**





# Longview

## Creating Community



**As part of the Local Area Plan, new educational facilities for all ages will be provided, including a crèche, primary, and post primary schools.**

The new urban park within Longview, which extends over 16 acres, will contain playing pitches and a playground, and will be an important recreational facility for all ages.

These will be accessible from all clusters in the development, and their intended location towards the top of the hill will provide the best of views of the surrounding countryside for all to enjoy. This scheme aims to create a distinctive character and a definitive identity, with safe and user-friendly spaces throughout the site and good links to wider green infrastructure.



### Green Space at a Glance

The County Development Plan 2014 requires that 12% to 18% of a development site should be allocated to public open space.

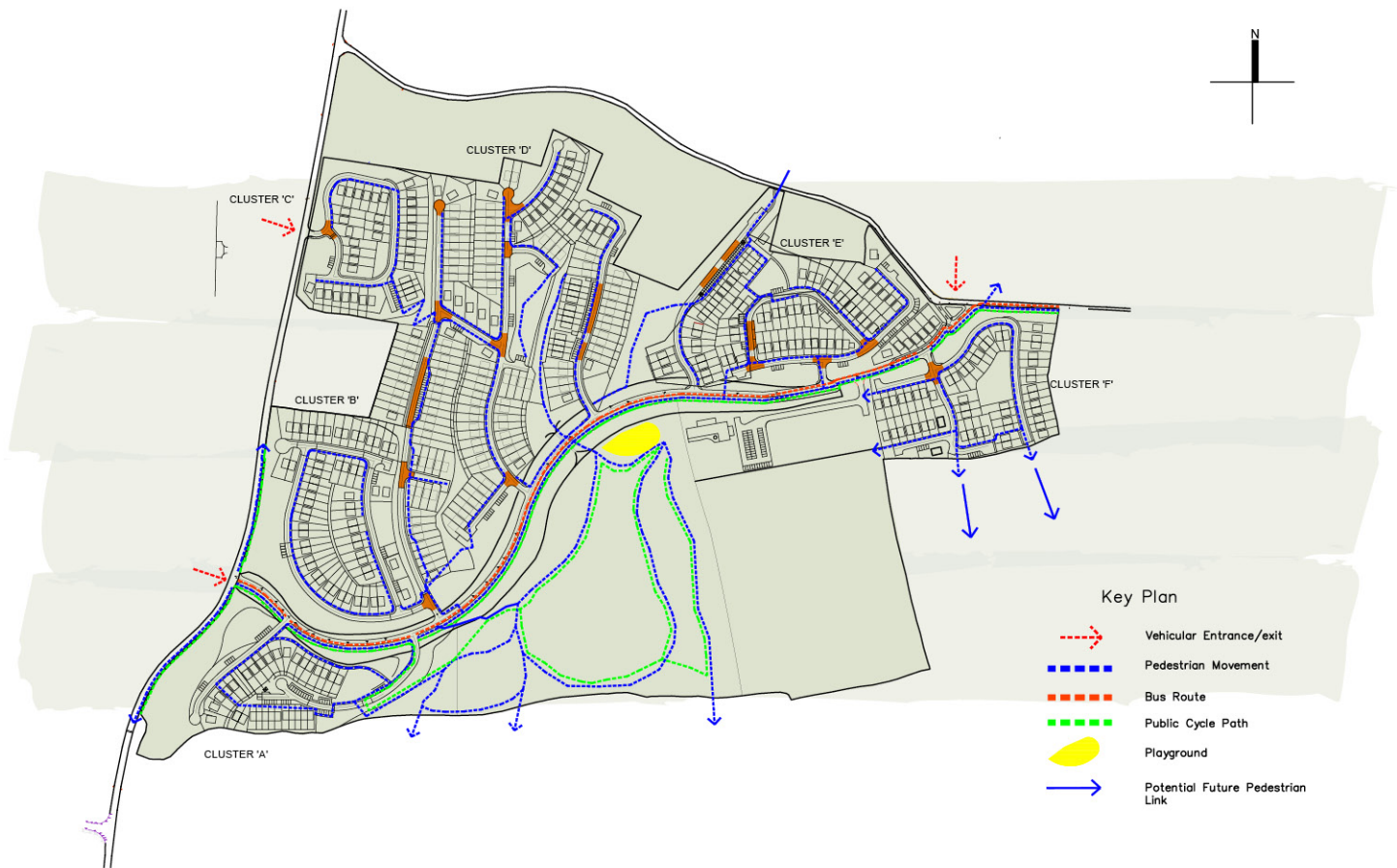
**For Longview we have exceeded standard ratios:**

- Usable % of open / green space zoned lands is **17%**.
- Usable % of open / green space zoned lands & park is **35%**
- Usable % of open / green space zoned lands, green belt & ancillary is **41%**



# Longview

## Connecting Communities



**Permeability, (ease of access between clusters and the park), for residents of Longview, along with external connectivity to the city are central facets of the design of this development.**

Extensive scenic cycle paths and landscaped pedestrian walkways will be provided throughout. The design incorporates a proposed footbridge crossing the central spine road, facilitating safe, unrestricted access to the

parkland and the avoiding the need to cross the road, (additional pedestrian crossings will also be provided).

Furthermore, bus routes will be extended through Longview to ensure ease of access to the city centre while also servicing the proposed schools in the area. Bus routes will operate through the three strategic transport corridors in the immediate area, those being Blackpool, Ballyvolane, and Mayfield.

**“A dedicated bus lane with frequent services will provide residents with access to employment locations in the City Centre, the South West and South East of the City. This will also enhance the connectivity to the proposed schools...”**



# Longview

## Construction Management



The goal during the phased construction period is for Longview to have as minimal an impact on the land and on adjacent land owners as possible.

To achieve this, Longview Estates Ltd. is committed to providing and adhering to a detailed Construction Management Plan that will take the following into consideration:

- Waste management and waste segregation procedures
- Surface water run off
- General construction works and construction sequence
- Site hoarding and signage
- Cleaning of road and vehicles
- Traffic plan, deliveries, construction access, and parking
- Contractor's compound maintenance and operation
- Hours of work
- Dust, noise & vibration minimisation

**“Health & Safety onsite is of paramount importance, and ensuring the safety of all staff onsite, and all residents in the surrounding area is a matter we will treat as our utmost priority”**





# Longview

## House Design & Sample Images



**A range of 20 home types have been designed for Longview that will create variety and diversity throughout the clusters.**

High-quality, long-lasting materials will be used for construction and finishes, with the intention of creating a benchmark standard for homes in the area. Internally, the proposed 2.7m floor to ceiling provides an opportunity for higher than average windows, generally resulting in additional natural light internally and increased passive solar gain.

Single-storey extensions to the rear of dwellings ensure that glazing is facing in an easterly, southern, or westerly direction.

# Longview

## Energy Efficiency

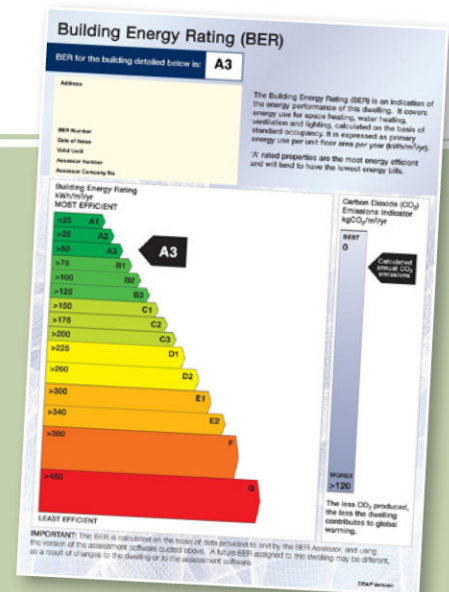


**Each home will be designed to maximise thermal insulation of floors, walls and roofs.**

Management of air tightness will be a priority, while also ensuring that adequate and high standards of natural ventilation are maintained. Our goal is that all homes in Longview will achieve a BER rating of A3 at a minimum. The detailed design for future-proofing homes in Longview is currently considering:

- Air to water heat pumps
- Mechanical heat recovery ventilation
- Incorporation of passive build techniques (greatly reducing energy bills)
- Photo-voltaic solar power
- Electric car charging points

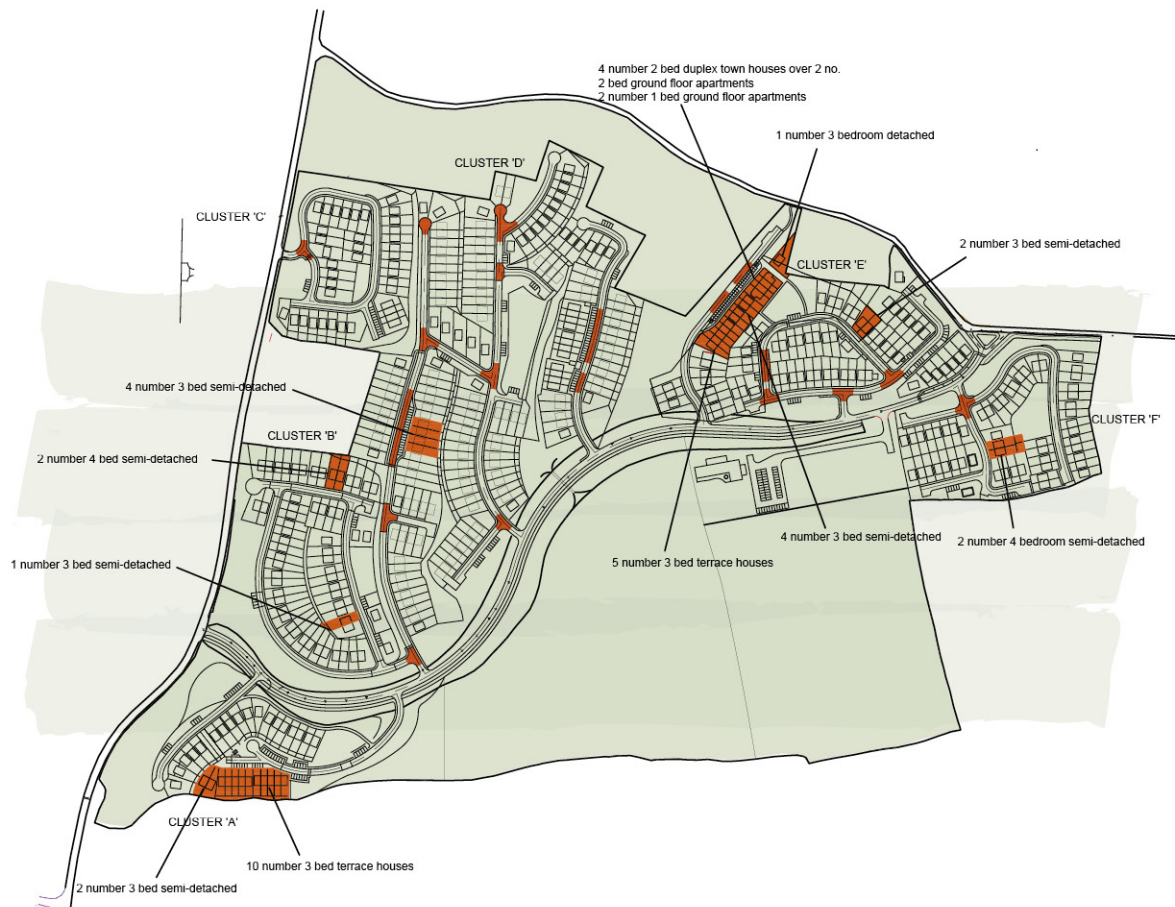
**“Our goal is that all homes in Longview will achieve a BER rating of A3 at a minimum, in order to ensure that the costs of running these homes will be as low as possible.”**





# Longview

## Social & Environmental



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Consequently the development will provide over 40 social houses in line with Part V policy.

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The planting of mature native tree species and preservation of existing hedgerows will be a central aspect of the landscaping, which will enhance appearance and the ecological sustainability in the area.

A comprehensive Environmental Impact Assessment, (EIA), has also been undertaken.

**“No rare or protected plants and no invasive non-native plants species were recorded on site. There was no evidence of protected species of amphibians, invertebrates or non-volant mammals at the site...”**



# Longview

## Residents Q&A

**Longview Estates Ltd. is aware that this development may cause concerns for local residents, and understands the importance of engaging with you in order to mitigate these concerns in the best possible way.**

**Q.**

**Will this development increase traffic congestion in the area?**

**A.**

Longview Estates Ltd. has engaged a specialised team of engineers to assess potential future impacts on traffic in the vicinity. Seven junctions have been assessed for traffic impact from 2017 to 2024, with a range of transport solutions discussed at Council level.

It is important to note that the trunk road to the north of Cluster F will not be accessible to the houses in the development until the final phase has been completed (estimated 2023), and until after the completion of the future Mayfield link road.

**Q.**

**How will the construction period impact the local area?**

**A.**

Longview Estates Ltd. will develop a detailed Construction Management Plan which will endeavour to minimise disturbance to the local area.

**Q.**

**Will my transport options be improved?**

**A.**

As part of the development, bus connectivity in the area will be improved with additional links to all the strategic corridors of Blackpool, Ballyvolane, and Mayfield.

There will also be access to new pedestrian walkways and cycle paths. Additionally the development will provide new transport routes to residents, reducing the flow into pinch-point junctions.

**Q.**

**Will public amenities be improved in the area?**

**A.**

The provision for a crèche, primary and post-primary schools are included in the community zoned area of the development. These will serve Longview and the Ballyvolane Urban Expansion Area.

Longview also plans to build a sports field situated in the park. Additionally, existing water, telecommunications and sewage infrastructure to the area will be upgraded. This investment will begin wider development, which in turn could lead to increased commercial services becoming available.

**Longview Estates Ltd. wish to thank you for your attendance at this event and for sharing your feedback. We look forward to working with the residents throughout this exciting development.**

**Please feel free to contact Longview Estates Ltd. at [info@longview.ie](mailto:info@longview.ie) with your questions or any feedback.**





# Longview

Aerial Image



Artist's impression of the overall development



**Longview**

[www.longview.ie](http://www.longview.ie) [info@longview.ie](mailto:info@longview.ie)

## **Appendix C** Key Records of Consultation relevant to the Proposed Development





**John Crean  
Longview Estates Ltd,  
Unit 74,  
Penrose Wharf,  
Penrose Quay,  
Cork.  
T23 HF51**

06 November 2019

Ref: Proposed development at townland Ballincolly & Lahardane  
Our Ref: Proposed line diversion on Kilbarry-Flaxfort-Mayfield 38kV line

Dear John,

Thank you for your application for an alteration to our network at the above location. Based upon information provided (drawing no.LHD-ESBD-PO1) by you to Electricity Supply Board Networks (ESBN) will agree to carry out the requested changes to the distribution network, see attached map of proposed line diversion on Kilbarry-Flaxfort-Mayfield 38kV line and drawing of lattice steel mast type 54D 12 metre subject to acceptance of Terms and conditions.

I have met with Mr John Crean at the proposed development and we have discussed the safety issues arising from the proximity of the 38Kv overhead electricity line to the proposed development.

ESBN has no problem in altering the overhead line to facilitate the construction of the development and John has agreed to cooperate fully with the ESB to achieve this.

The ESB does not have an issue with work commencing on site prior to the 38kV line being altered once the precautions outlined in the Code of Practice for Avoiding Danger from Overhead Electricity are implemented on site.

I will be working closely with John and his contractor on an ongoing basis.

If you require further information or if I can be of assistance in any way regarding this matter, please do not hesitate in contacting me.

Yours Sincerely

Pat Harrington  
Design Office 38KV Dept  
Network Project South  
Sarsfield Road  
Wilton  
Cork  
Ext 54205 DDI 021- 4844205  
Mob.087-2792843



## Networks Ducting/Cabling (Minimum Standards)

Note 1 : ESB Networks reserves the right not to accept ducting which does not conform to these standards and dimensions

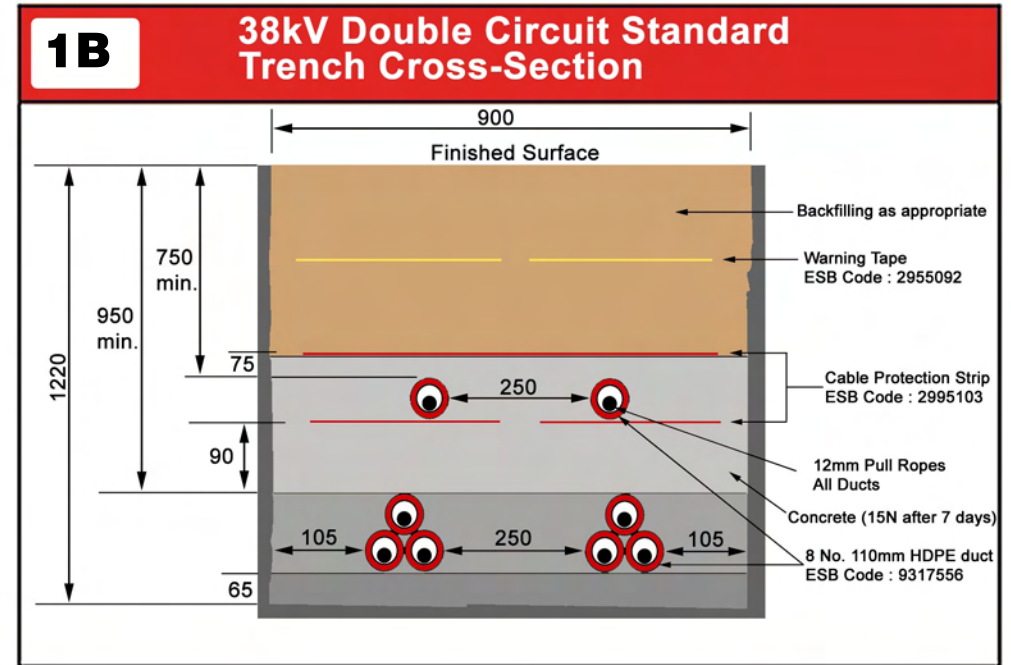
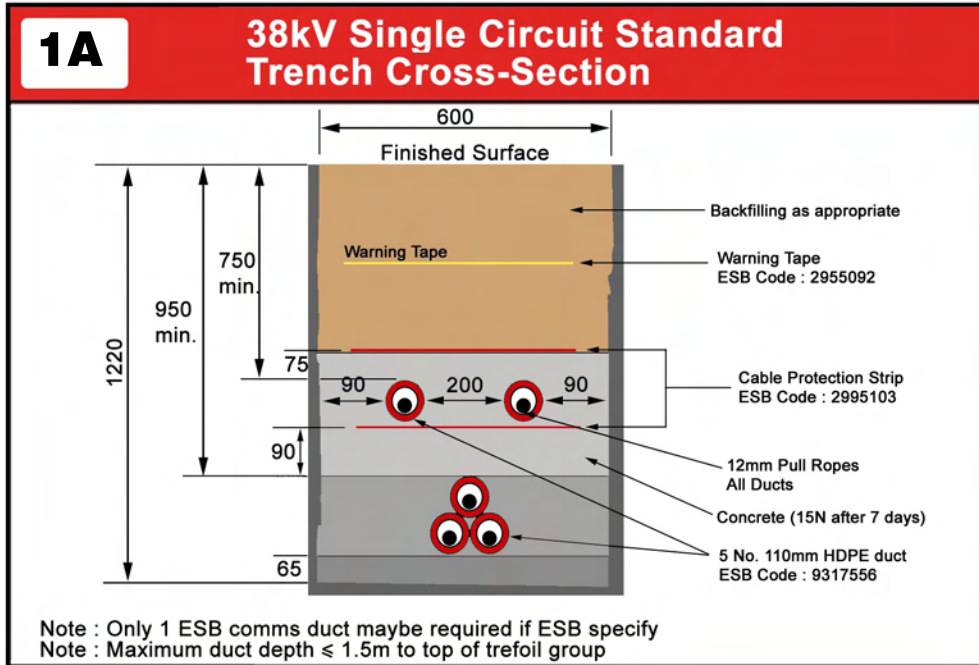
Note 2 : Refer to ESB Networks for Specific job Specification. These instructions do not apply to LV/MV/110kV/220kV cable

Note 3 : All materials (ducts, marker tapes/strips, duct surrounds, mandrels and brushes) must be ESB approved materials

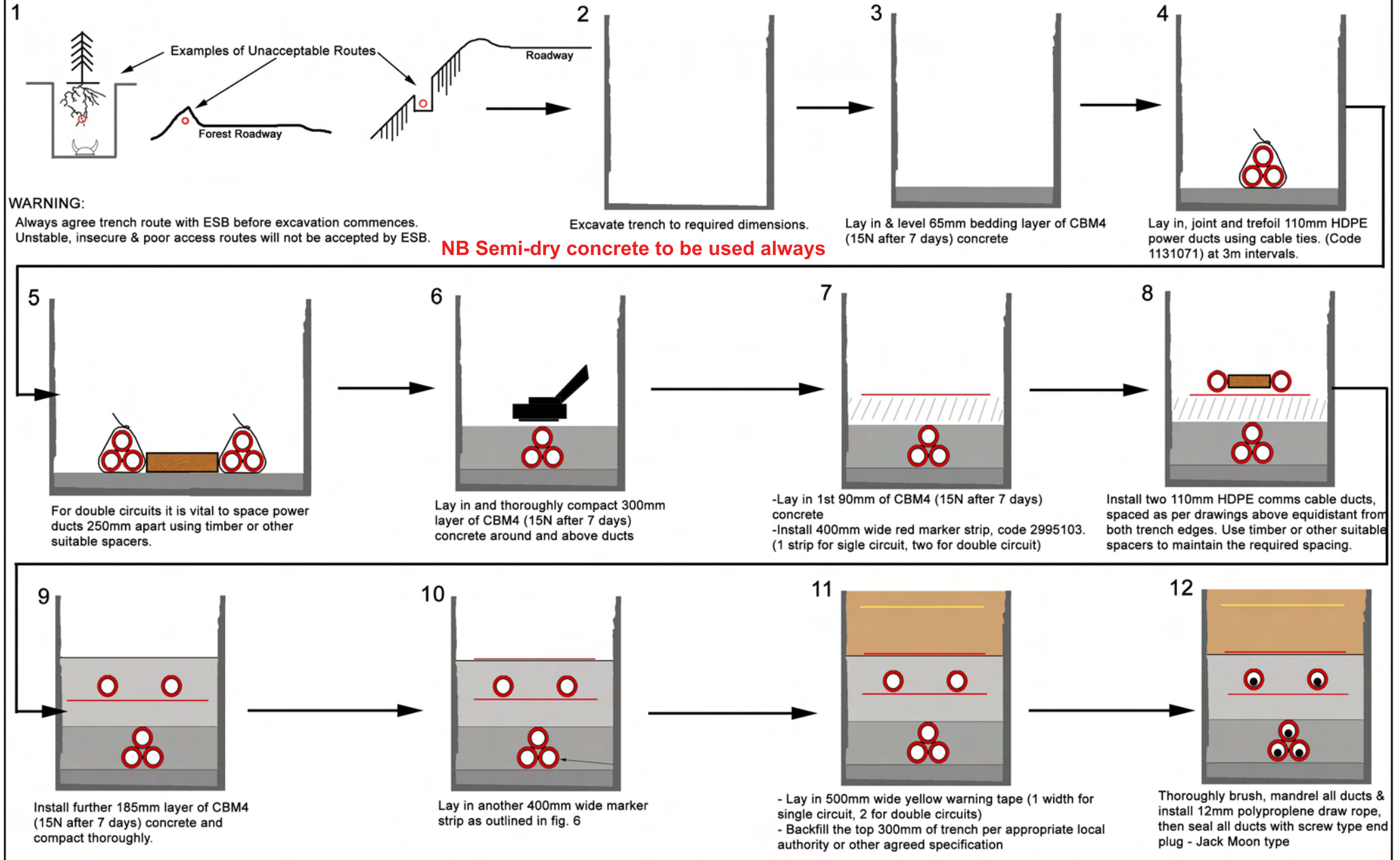


Rev 0: Date 08-09

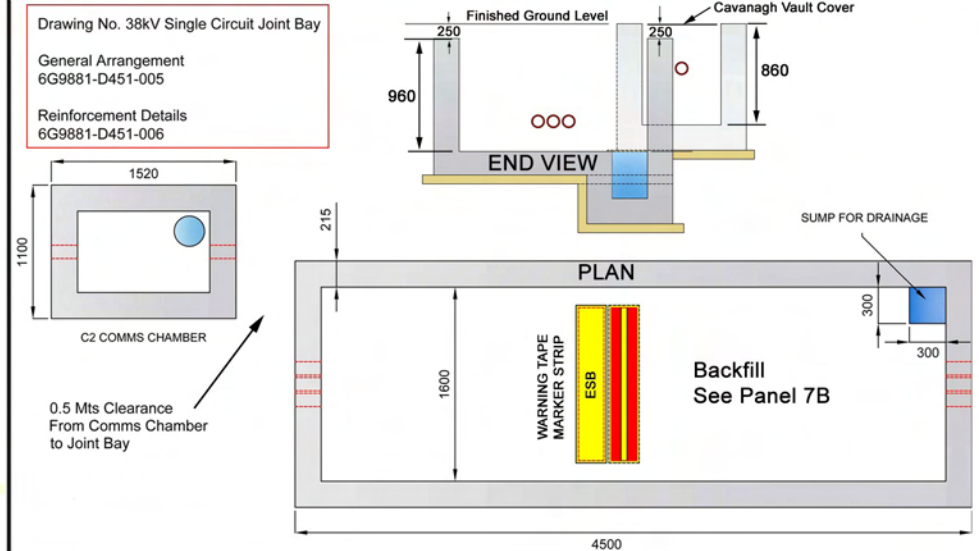
Approved:



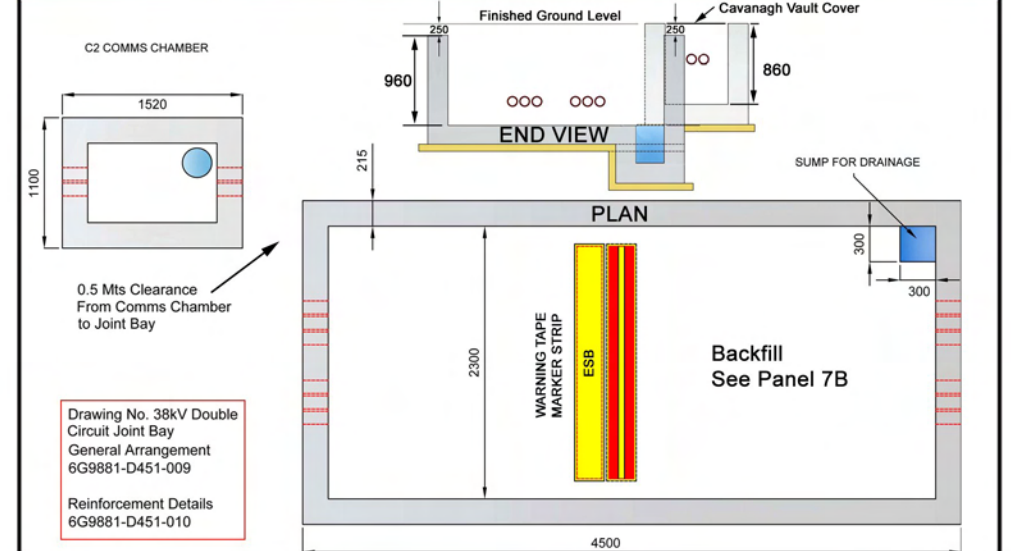
### 1C Trench Installation Sequence



### 2A 38kV Single Circuit Joint Bay



### 2B 38kV Double Circuit Joint Bay





## Networks Ducting/Cabling (Minimum Standards)

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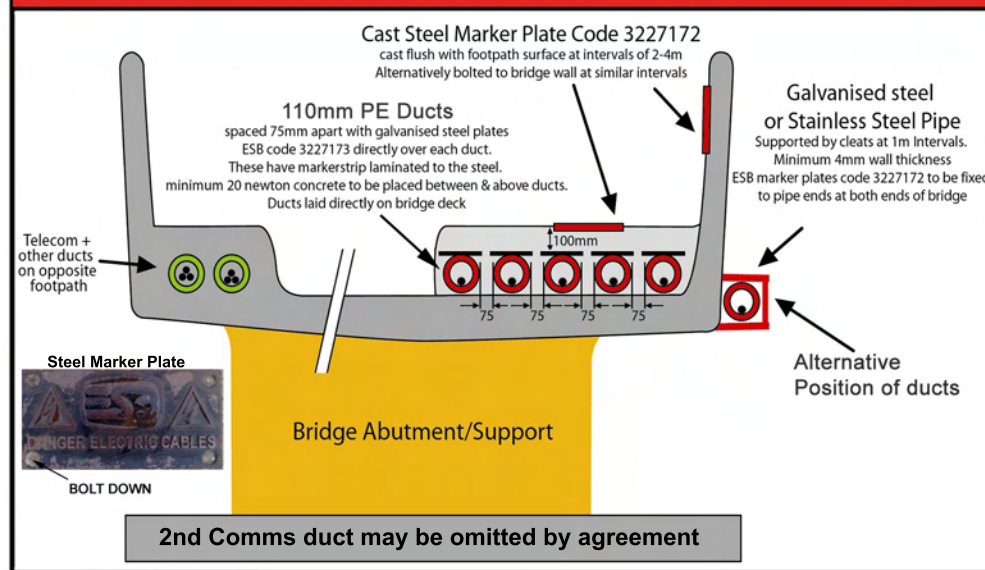
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Rev 0: Date 08-09

Approved:

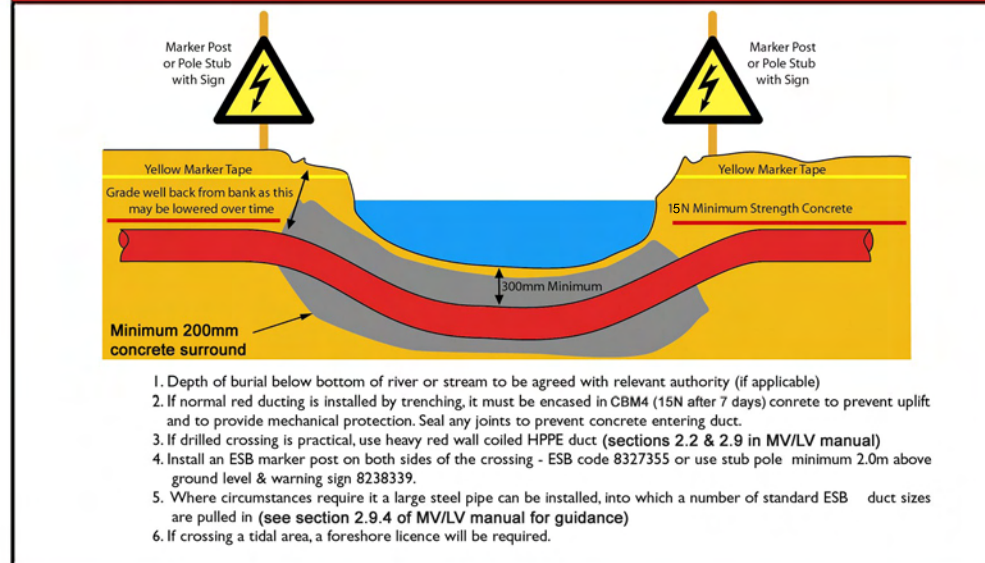
### 5A Bridge Crossings: Restricted Footpath Designs



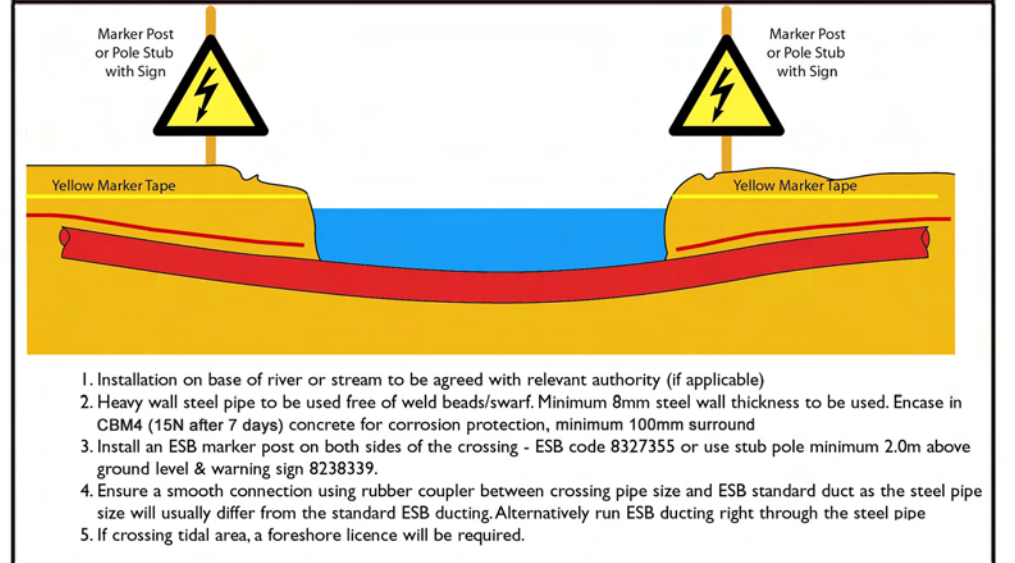
### 5B Bridge Crossings: Restricted Footpath Designs

1. The design must be agreed with the bridge authority. Position in footpath is preferred.
2. Minimum cover over ducts on footpath 100mm.
3. Where duct cover is > 300mm, marker strip & surface marker plates can be used.
4. Red ducting is not suitable for cable run external to bridges.
5. Where possible galvanised steel/stainless steel piping should be used, all joints must be free of weld burrs on inside. Alternatively heavy duty 10mm wall thickness black HDPE material with cast steel marker plates attached must be used to permanently warn of presence of electric cable.

### 6A River/Stream Crossings: Standard Where Burial/Drilling IS Possible



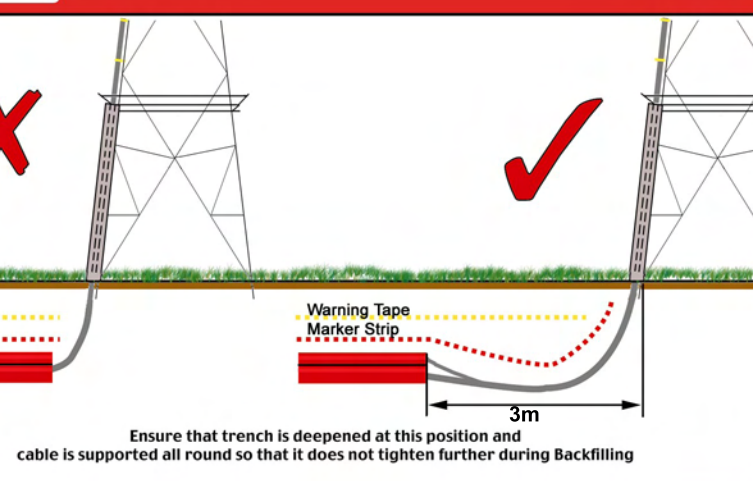
### 6B River/Stream Crossings: Standard Where Burial/Drilling IS NOT Possible



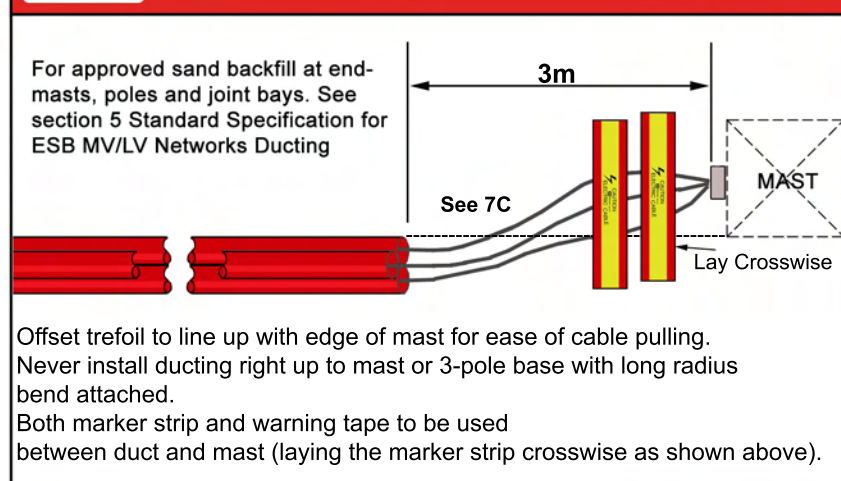
1. Depth of burial below bottom of river or stream to be agreed with relevant authority (if applicable)
2. If normal red ducting is installed by trenching, it must be encased in CBM4 (15N after 7 days) concrete to prevent uplift and to provide mechanical protection. Seal any joints to prevent concrete entering duct.
3. If drilled crossing is practical, use heavy red wall coiled HPPE duct (sections 2.2 & 2.9 in MV/LV manual)
4. Install an ESB marker post on both sides of the crossing - ESB code 8327355 or use stub pole minimum 2.0m above ground level & warning sign 8238339.
5. Where circumstances require it a large steel pipe can be installed, into which a number of standard ESB duct sizes are pulled in (see section 2.9.4 of MV/LV manual for guidance)
6. If crossing a tidal area, a foreshore licence will be required.

1. Installation on base of river or stream to be agreed with relevant authority (if applicable)
2. Heavy wall steel pipe to be used free of weld beads/swarf. Minimum 8mm steel wall thickness to be used. Encase in CBM4 (15N after 7 days) concrete for corrosion protection, minimum 100mm surround
3. Install an ESB marker post on both sides of the crossing - ESB code 8327355 or use stub pole minimum 2.0m above ground level & warning sign 8238339.
4. Ensure a smooth connection using rubber coupler between crossing pipe size and ESB standard duct as the steel pipe size will usually differ from the standard ESB ducting. Alternatively run ESB ducting right through the steel pipe
5. If crossing tidal area, a foreshore licence will be required.

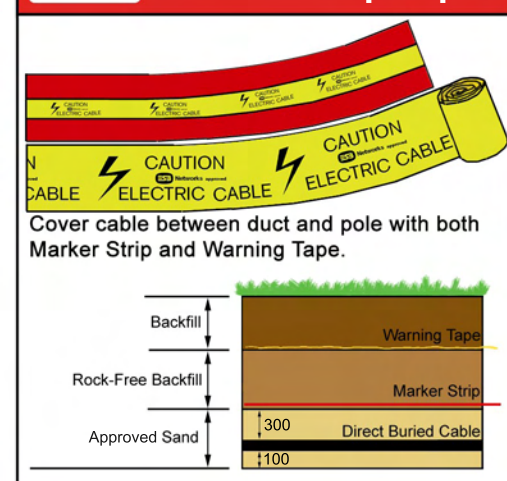
### 7A Cable End Mast Position



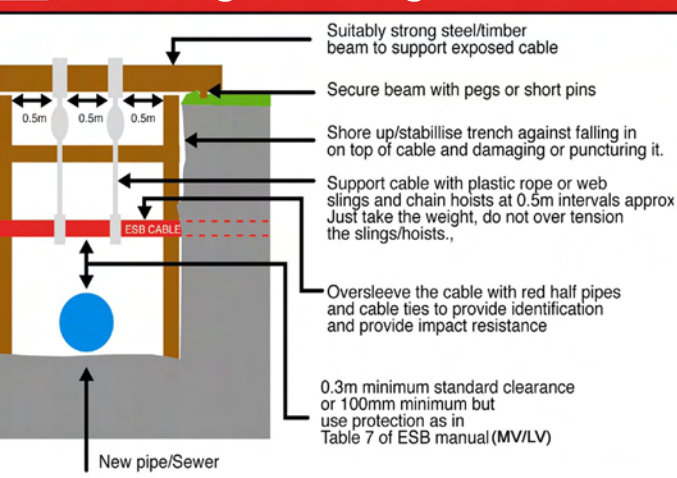
### 7B Cable End Mast Position



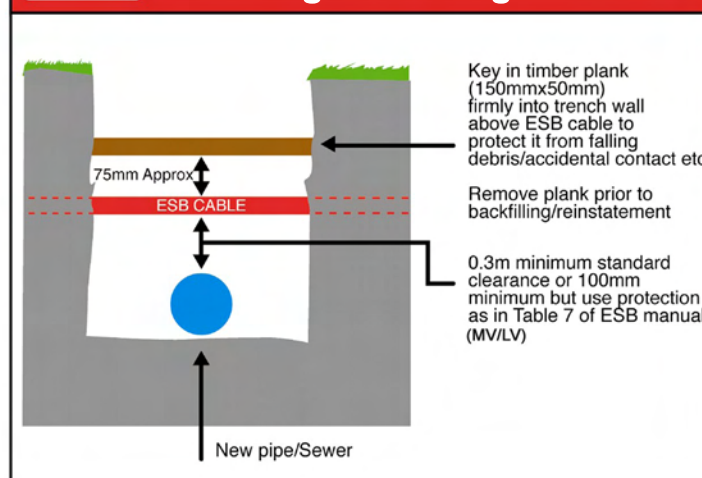
### 7C Cable End Mast - Marker Strip/Tape



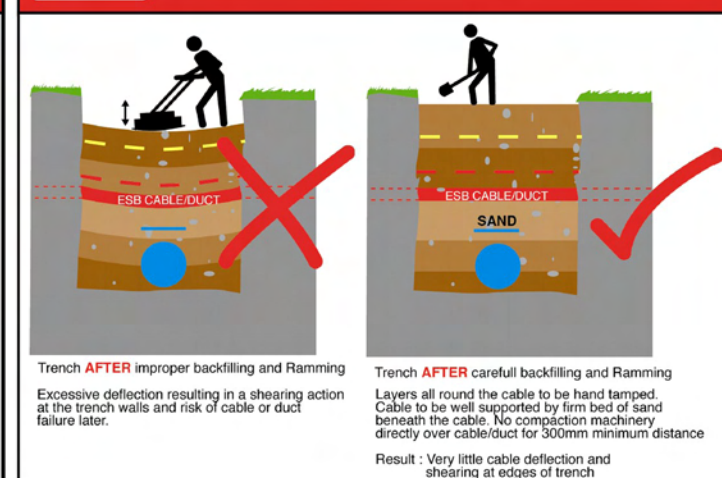
### 8A Supporting ESB Cables/Ducts During Trenching Works



### 8B Supporting ESB Cables/Ducts During Trenching Works



### 9 Avoidance of Cable Damage Due to Improper Backfilling at Cable Crossings





## Networks Ducting/Cabling (Minimum Standards)

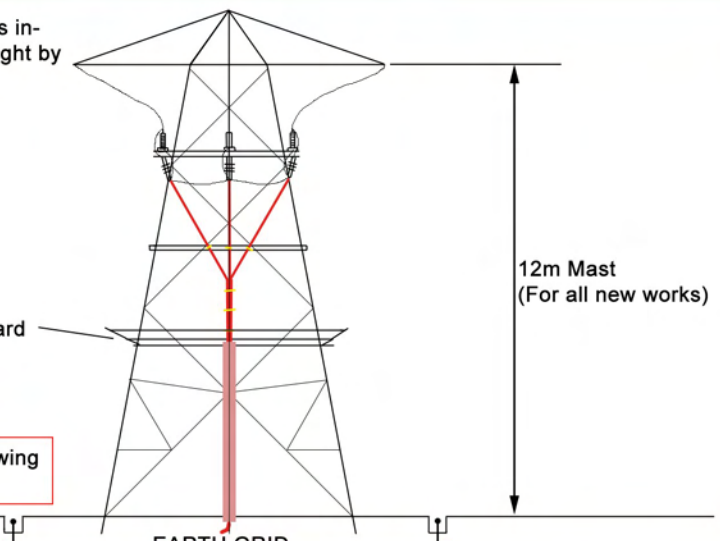
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 Note 3 : All materials (ducts, marker tapes/strips, duct surrounds, mandrels and brushes) must be ESB approved materials



Rev 0: Date 08-09  
 Approved:

### 3A End Mast Termination

For existing 9m masts increase steel work height by 1.3m at mast top




12m Mast (For all new works)

Anti-Climbing Guard

EARTH GRID

Cable Assembly Drawing Number : D205778

### 3B Triple Pole Structure




Cable Steel Work Code: 1286697

Made up anti-climbing guard

7m Min Dimension to Bare Metal Use 12m Pole

### 3C Station Termination



To Cubicle

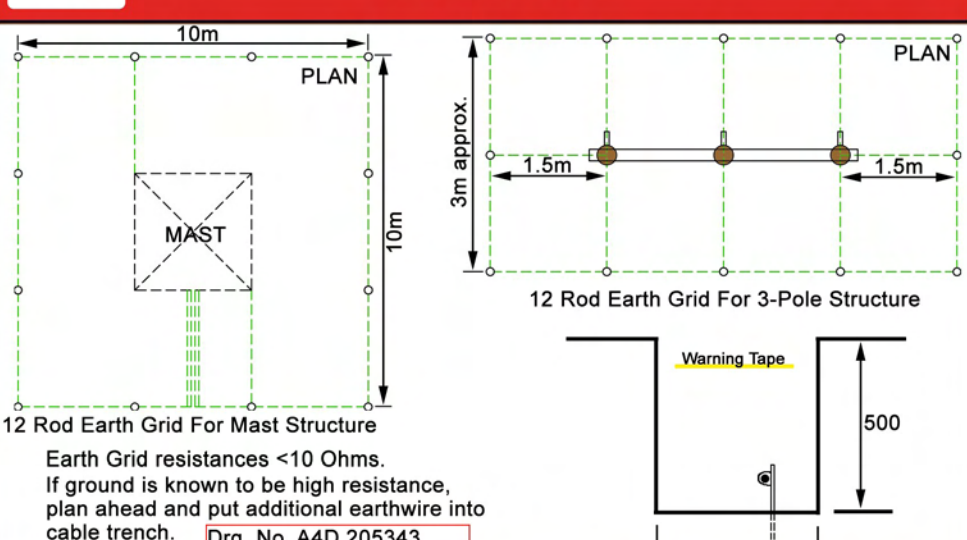
If Cable run <50m install lightning arrestors.

Assess need for mesh screen guard (Code: 3175003)

Drg. No A3205856

Clearances : Phase to:  
 - Phase 500mm outdoor  
 - Earth 500mm outdoor

### 3D Earth Grids



10m PLAN

10m

3m approx.

1.5m

1.5m

12 Rod Earth Grid For 3-Pole Structure

Warning Tape

500

300


12 Rod Earth Grid For Mast Structure

Earth Grid resistances <10 Ohms. If ground is known to be high resistance, plan ahead and put additional earthwire into cable trench.

Drg. No. A4D 205343  
 PE424-D901-911-001-000

### 4A Obligation of Duct Installer to minimise the number and severity of duct bends

The duct installer must minimise the number and severity of preformed bends in ground with obstructions and other utility service crossings by opening ground 15m ahead of backfilled duct, wherever practical to do so. This safety obligation, which may require use of steel plating, allows the duct installer to pick the least bendy duct route through utility crossings and obstructions. Otherwise, numerous sharp unrecorded duct route deviations will be present making cable installation considerably more difficult and less safe for the cable installer.



Backfilled Duct

Obstructions

Digger

Dig 15m Ahead of duct to uncover obstructions

### 4B Standard for Brushing, Mandrelling, Roping and End-Capping of 38kV ducts

All Ducts must be:

- Thoroughly brushed and mandrelled to prove ducts against debris /excessive deflection
- Roped using 12mm polypropylene rope with certified safe breaking load of 1.5 tons – all rope joints to be properly spliced and PVC taped over. Approved Supplier Silver Strand Bunclana Donegal, ph (074) 9382503 - 500m drum lengths available to minimise splicing/coil handling
- Sealed using endcaps against grit and water getting into them
- NB: Replace mandrels once mandrel wear indicators or grooves are worn down
- Replace brushes once brush diameter falls 5mm below dimensions in table below
- Approved endcaps, both disposable and reusable types, are available from suppliers of approved ESB ducting
- Approved ESB Mandrel and brush suppliers :

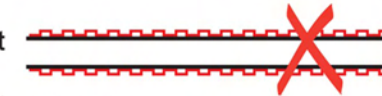
Brandon Agencies, Rathnew, Co Wicklow: Phone 0404 20500 (Brushes & Mandrels)  
 IS Varian, Greenhills industrial Estate, Walkinstown, Dublin 12 Phone: 01-4501150 (Brushes Only)  
 Clydesdale UK Phone 086 172 6665 (Brushes & Mandrels)  
 Tynagh Network Systems, Loughrea, Co Galway. Phone: 091 842206 (Brushes & Mandrels)

110mm HDPE Duct Size

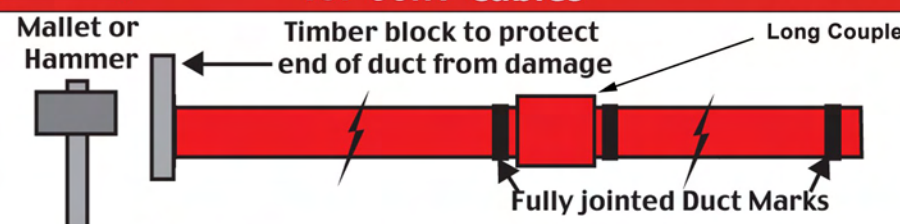
85mm	250mm	250mm
Mandrel	Brush	Sponge
Code: 9317546	Code: 8783255	Code: 8783252

### 4C Approved ESB Ducting for 38kV Cables

- Use only solid wall high impact resistance ESB approved HDPE red ducting to IS 370 colour standard and ESB specification 16113 (6.3mm minimum wall thickness) Discoloured or unidentified ducting not acceptable. All duct material must be approved by ESB Networks.
- Lightweight flexible corrugated twinwall ducting is not acceptable to ESB irrespective of manufacturer
- Current approved HDPE Duct and duct bend manufacturers are: Lynplast (bend fittings only), Uponor-Radius Systems, Wavin, Quality Plastics



### 4D Specification for Duct Jointing for 38kV Cables



Mallet or Hammer

Timber block to protect end of duct from damage

Long Coupler

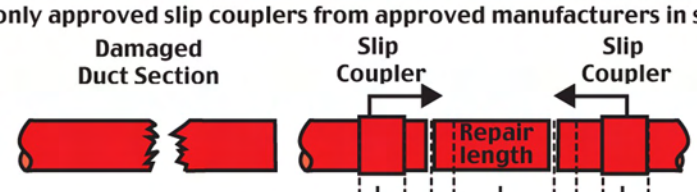
Fully jointed Duct Marks

All ducts to be securely jointed by tapping against timber board on each duct until the black depth insertion mark is reached

Always smear duct lubricant on coupler rubber ring

### 4E Repair of Existing Ducts

Use only approved slip couplers from approved manufacturers in section 4C



Damaged Duct Section

Slip Coupler


Slip Coupler

Repair length

- Cut out damaged section of duct and ensure all cut surfaces are square and free from sharp edges
- Slide, position and centre the repair couplers on the centering marks

### 4F Sealing of Ducts

All ducts to be permanently sealed at both ends of duct run  
 Ducts to be temporarily sealed during installation using endcaps provided with each bale



Endcap Plain End

ESB Code 110mm: 9317569



### 10A 38kV Railway Crossing Details

ESB Signpost

3m

Drilling pits outside CIE property line

Formal licence for crossing and approval required from CIE. Accurately record crossing location & erect marker posts.

### 10B Directional Drill/Thrust Bore Duct Bore Details

DESIGN 1

Minimum internal bore size = 325mm for 5 ducts

=290mm for 4 ducts where approved by ESB

5 no. 110mm diameter HDPE ducts

Alternatively use 2 x 37mm HDPE ducts for comms cables with C2 chamber on each side of the crossing to permit pulling along entire route. (See 10C)

Completed interstitial space to be bentonited thoroughly to maintain cable rating. Accurately record crossing location & erect marker posts.

### 10C Directional Drill/Thrust Bore Duct Bore Details

ALTERNATIVE DESIGN

ESB Signpost

3m

Cable joint pit

Install 1 no. 200mm SDR 17.6 duct with 3 no. short length cables pulled into this pipe along with 2 x 37mm comms ducts. Full cable joint bays are required on either side of crossing along with C2 chambers for this design. This method is used where it is not practical to install large diameter pipe -eg. risk of ground upheaval or presence of obstructions. Completed interstitial space to be thoroughly bentonited to maintain cable rating. Accurately record crossing location & erect marker posts.

### 10D Double Circuit Bore Crossing

Standard Design

3m min

-Both Bentonited

Separate drilling for each circuit crossing

Alternative

HDPE or steel thrust bore pipe Diameter ID= 400mm

6 no. 110mm Power ducts + 2 no. 110mm comms ducts

2 no. sets of 110mm HDPE ducts - 8 ducts in total. All crossings to be accurately recorded and signposts erected given impracticality of marker tape. If both circuits = 40MVA then use 630 Cu cable

### 12 Minimum Standard Clearances to Other Services

Normal Services 300

Large Pipelines High Pressure Pipes 600

Clearances less than the above at pinch points and crossings requires placement of additional mechanical protection (concrete slab/brick) and agreement of ESB

ESB ducts must never be laid over other services on parallel runs, except with the written prior agreement of the other utilities and ESB

Other services must never be laid directly over ESB ducts on parallel runs

### 13 Combined MV & 38kV Cable Runs

38kV Trench

MV/LV Cables

1.1m to 1.25m Depending on Location

Yellow Marker Tape

Red Marker Strip

Pilot Cables

Concrete Surround

150mm

150mm

Additional MV/LV Ducts as Required

300mm Strict Minimum Separation

Where it is impractical to avoid such trench runs, the separation of 300mm should be strictly controlled and monitored to minimise derating (See MV/LV manual page 180)

### 14 Sealing and Protection of 38kV Cables Once They Exit Ducts

Duct

Ducts to be thoroughly using ESB approved water sealant and 4hr fire rating approved for firestop. NB - All joint bay duct entries to be thoroughly sealed to prevent sand washout and subsidence.

Sandbags or other durable support for cable as it exits ducts to prevent damage to cable sheath

### 15 Duct Crossovers Are Not Allowed

Be especially careful when going from flat to trefoil formation in vicinity of services

Eliminate this possibility by marking ducts 1, 2, 3 etc before & after flattening to avoid an obstruction.

NB. If using double circuit, tape mark power ducts 1 to 6

### 16 Crossing Dumps/Contaminated Ground

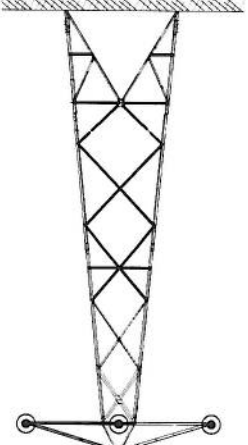
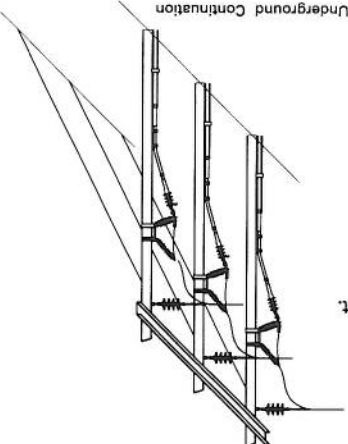
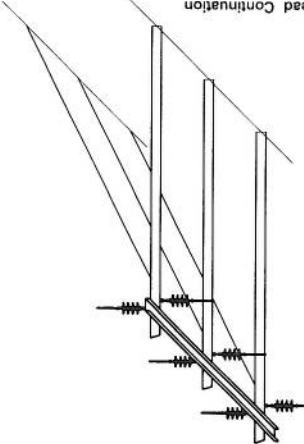
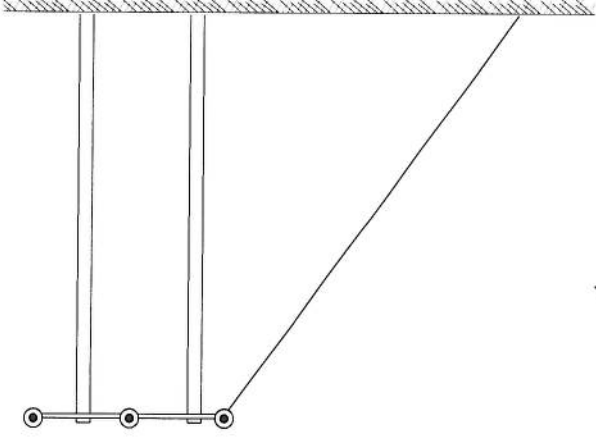
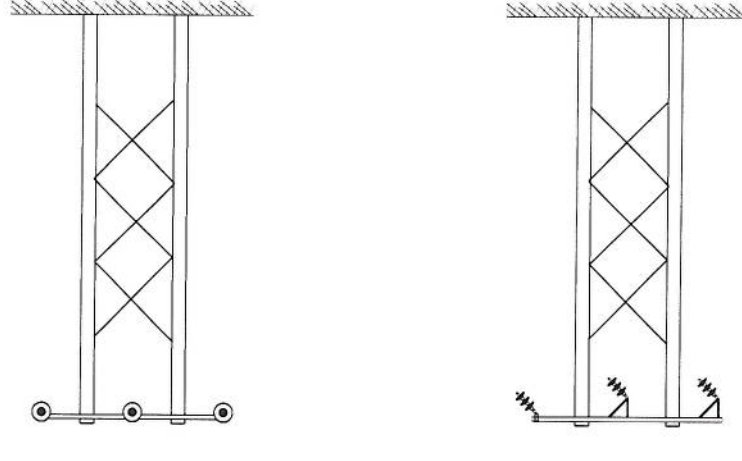
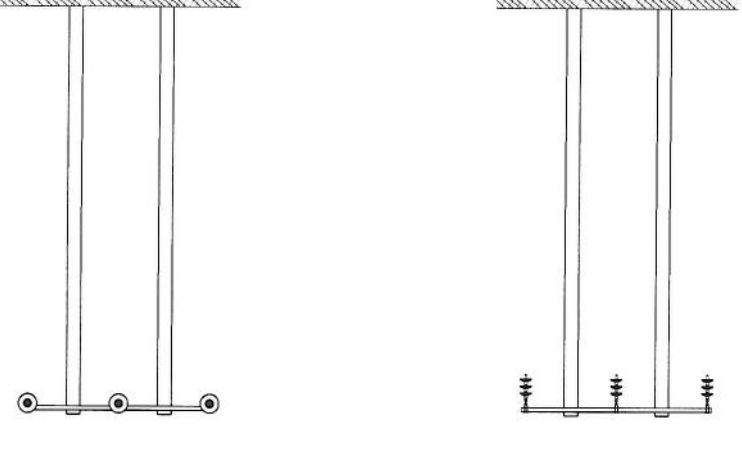
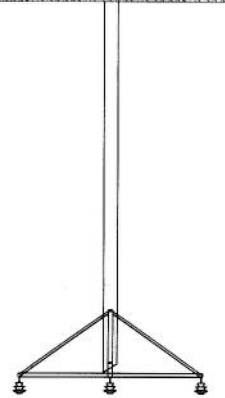
Thoroughly seal all joints with adhesive water-tight duct jointing compound and pressure test for airtightness. Gasketed couplers alone are inadequate. Fusion welded couplers are also acceptable but require red over-taping.

NB. Avoid whenever possible due to: Subsidence, methane gas & severe thermal derating risks. Seek advice from ug networks section to ensure rating of cable is adequate (derating of 50% can occur) NB. Waste oils and chemicals can also seriously damage cables

Seal all duct joints with duct adhesive compound or use continuous duct lengths & seal all duct ends in joint bays. Alternatively weld pipes.

Concrete is continued up to 300mm of final surface to offset derating (CBM4 - 15N after 7 days)



<p>FOR SCALED 1:100 DRAWINGS, SEE SHS 002-007</p>  <p>Type "F" Lattice steel mast Structure.          Base buried depth 2.3m. in concrete.          Maximum conductor height overground 9, 12 or 15m.          Corresponding base footprint 2.4, 2.8, or 3.2m.          Mast apex can be up to 1m over conductor height depending on manufacture.          4m crossarm with horizontal insulator chain holding conductors 2m apart.          Bracing shown may change depending on manufacture.          Can be used as an End mast to terminate a line, or used as an Angle mast to allow up to a 60deg. deviation in the line.</p>	<p>Type "E" Three-Pole Structure          Three wood poles, 2m apart with a 4m steel crossarm holding conductors horizontally 2m apart.          Insulator chain is horizontal.          Pole lengths from 12m to 16m incl. buried depth 2.3m.          Maximum conductor height overground 13.4m          Three staywires, (one on each pole) extend from pole tops at 45deg.</p>  <p>Underground Continuation</p>  <p>Overhead Continuation</p>	<p>Type "D" Heavy Angle Portal Structure          Two wood poles, 2.5m apart with a 4m steel crossarm holding conductors horizontally 2m apart.          Insulator chain is horizontal.          Pole lengths from 12m to 16m incl. buried depth 2.3m.          Maximum conductor height overground 13.6m.          Line can deviate by 60deg. with two staywires extending from the crossarm at 45deg.</p> 	<p>Type "C" Light Angle Suspension &amp; Light Angle Strain Structures          Two wood poles, 2m apart with a 4m steel crossarm holding conductors horizontally 2m apart.          Insulator chain may be in suspension or horizontal.          Pole lengths from 12m to 16m incl. buried depth 2.3m.          Maximum conductor height overground 13.2m.          Line can deviate by 20deg. with up to 3 sets of cross bracings.</p> 	<p>Type "B" Portal Suspension &amp; Portal Strain Structures          Two wood poles, 2m apart with a 4m steel crossarm holding conductors horizontally 2m apart.          Insulator chain may be vertical or horizontal.          Pole lengths from 12m to 20m incl. buried depth 2.3m.          Maximum conductor height overground 17.2m.          Poles can also be 3m apart with a 6m steel crossarm holding conductors horizontally 3m apart.</p> 	<p>Type "A" Single Pole Intermediate Structure          Single wood pole with 4m steel crossarm holding conductors horizontally 2m apart.          Pole lengths from 12m to 16m incl. buried depth 2.3m.          Maximum conductor height overground 14.1m.</p> 
---	--	--	---	--	---

Rev  
 1 STRUCTURE TYPE "F" ADDED  
 0 IMPORTED FROM ESB NETWORKS AT REV 0  
 Revision Description  
 Purpose of issue - Preliminary unless indicated  
 Tender  Client Approval  Construction  As-built  Revised



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 www.esb.ie  
 Registered Office as above  
 Registered in Ireland No. 155249

Client  
 Project  
 Contract

GENERIC LINES DOCUMENTS

N/A

Production Unit  
 High Voltage Engineering

Drawing Title  
 38KV LINES /PLANNING APPLICATION  
 OUTLINE OF TYPICAL STRUCTURES  
 SINGLE PHASE RACCOON & MULBERRY CONDUCTORS

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Drawn	Produced	Verified	Approved
E.B.L.	E.Lowlor	J.Eustoce	P.F.M.N.S.
Client Ref	TC205748		No. of Shis
Drawing Number	PG567-D020-213-001-001		10
		Size	Scale
		A3	N/A
		29/08/08	Approved date

SHEET REV

X,Y: 169786, 75377

PROPOSED  
END MAST  
TYPE 54D  
(12M)

PROPOSED  
END MAST  
TYPE 54D  
(12M)

17

18

19

20

21

22

PROPOSED CABLE ROUTE

630 AL XLPE  
3X110MM HDPE + 2  
1105 M





11

12

13

14

LEGEND

-  Proposed Lattice steel masts
-  Proposed underground cable route line
-  Existing 38kV Overhead line
-  Retire overhead line + poles & polesets

To be read with the attached structure drawing



SCALE: 1:2500

MAP NUMBER:6338-A

TOWNLAND:Ballincolly & Lahardane  
COUNTY:Cork

Project name:Proposed line diversion on  
Kilbarry-Flaxfort-Mayfield 38kV line

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X,Y: 168766, 74665

Note:structure symbols are not to scale







Energy for  
generations

esb.ie

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Engineering and Major Projects,  
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Phone +353 (0) 21 497 6300

Longview Estates Ltd  
Unit 74,  
Penrose Wharf,  
Penrose Quay,  
Cork.  
T12 HF51

**Our Ref: PE407-F072-1-9814**

**Proposed Housing Development at Lahardane, Ballyvolane, Cork in spans 12-15 of Kilbarry-Knockraha No. 1 110kV Line.**

Dear Mr. Crean,

We have reviewed the proposed development within spans 12-15 of the Kilbarry-Knockraha No. 1 110kV Line.

Based on drawing number 17-002-P-105, we have no objection with the proposed development. Buildings will not be constructed 25m either side of the centre wire of the 110kV line. In addition, works adjacent to poles as per drawing numbers LHD-ESBR-D01 & LHD-ESBR-D02 must be strictly adhered to ensure safety of the 110kV pole.

Access to the 110kV poles throughout the development must always be maintained, including when the development is complete. This access is required in the event of a fault and for maintenance purposes of the 110kV line.

Prior to any works taking place on site, can you please contact me to agree safe working procedures.

Please note that the above clearances are site specific and must not be used at any other location.

If you have any further queries, please don't hesitate to contact me.

Kind regards,

  
James Power  
ESB Engineering & Major Projects

## Orla O'Callaghan

---

**From:** Brian Geaney <brian\_geaney@corkcity.ie>  
**Sent:** 23 October 2019 20:11  
**To:** 'Liam De Barra'; John Crean  
**Cc:** Alison O'Rourke; Orla O'Callaghan; Liam DeBarra  
**Subject:** RE: Part V - Longview / Ballyvolane - Subject to Contract/Contract Denied

Liam,

The proposal as outlined is acceptable in principle to Cork City Council.

Regards,

Brian

---

**From:** Liam De Barra [mailto:Liam.DeBarra@temporiscapital.com]  
**Sent:** 23 October 2019 20:03  
**To:** John Crean; Brian Geaney  
**Cc:** Alison O'Rourke; Orla O'Callaghan; Liam DeBarra  
**Subject:** RE: Part V - Longview / Ballyvolane - Subject to Contract/Contract Denied

I'm  
Hi Brian,

I hope all is well.

Please find attached our proposed Part V Unit Mix for the purposes of the Planning Application and an overall map of the site with the proposed units identified.

As you may already know the proposed scheme consists of 753 units comprising of a mix of detached, semi-detached, terrace, duplex and apartments. As a result we are proposing a total of 75 units for our Part V obligation, comprising of the below;

17 x 2 Bed Ground Floor Duplex Apartments  
19 x 3 Bed Duplex Units  
11 x 3 Bed End of Terrace Units  
8 x 3 Bed Mid Terrace Units  
17 x 2 Bed Mid Terrace Units  
2 x 3 Bed Semi Detached Units  
1 x 4 Bed Semi Detached Unit

Summary;

1 x 4 Bed  
40 x 3 Bed  
34 x 2 Bed

I look forward to your feedback and hopeful acceptance for the planning application.

Mise le meas,

Liam.



---

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## Orla O'Callaghan

---

**From:** Brian Geaney <brian\_geaney@corkcity.ie>  
**Sent:** 21 March 2019 19:22  
**To:** 'John Crean'  
**Cc:** Alison O'Rourke; Orla O'Callaghan; Liam DeBarra  
**Subject:** RE: Part V - Longview / Ballyvolane - Subject to Contract/Contract Denied

John,

The City Council has no objection to the provision of duplex units as outlined below to meet Part V requirements for this proposed scheme, subject to the normal terms and conditions.

Regards,

Brian

---

**From:** John Crean [mailto:John.Crean@temporis.ie]  
**Sent:** 19 March 2019 10:12  
**To:** Brian Geaney  
**Cc:** Alison O'Rourke; 'Orla O'Callaghan'; Liam DeBarra  
**Subject:** Re: Part V - Longview / Ballyvolane

Brian

Just following up on this email. Can you advise if you would consider this duplex?

John Crean  
Temporis Limited, Unit 74, Penrose Wharf, Penrose Quay, Cork, T23 HF51  
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www.temporiscapital.com | Twitter | LinkedIn

---

**From:** John Crean <john.crean@temporis.ie>  
**Sent:** Tuesday, March 12, 2019 9:07 a.m.  
**To:** Brian Geaney  
**Cc:** Alison O'Rourke; 'Orla O'Callaghan'; Liam DeBarra  
**Subject:** RE: Part V - Longview / Ballyvolane

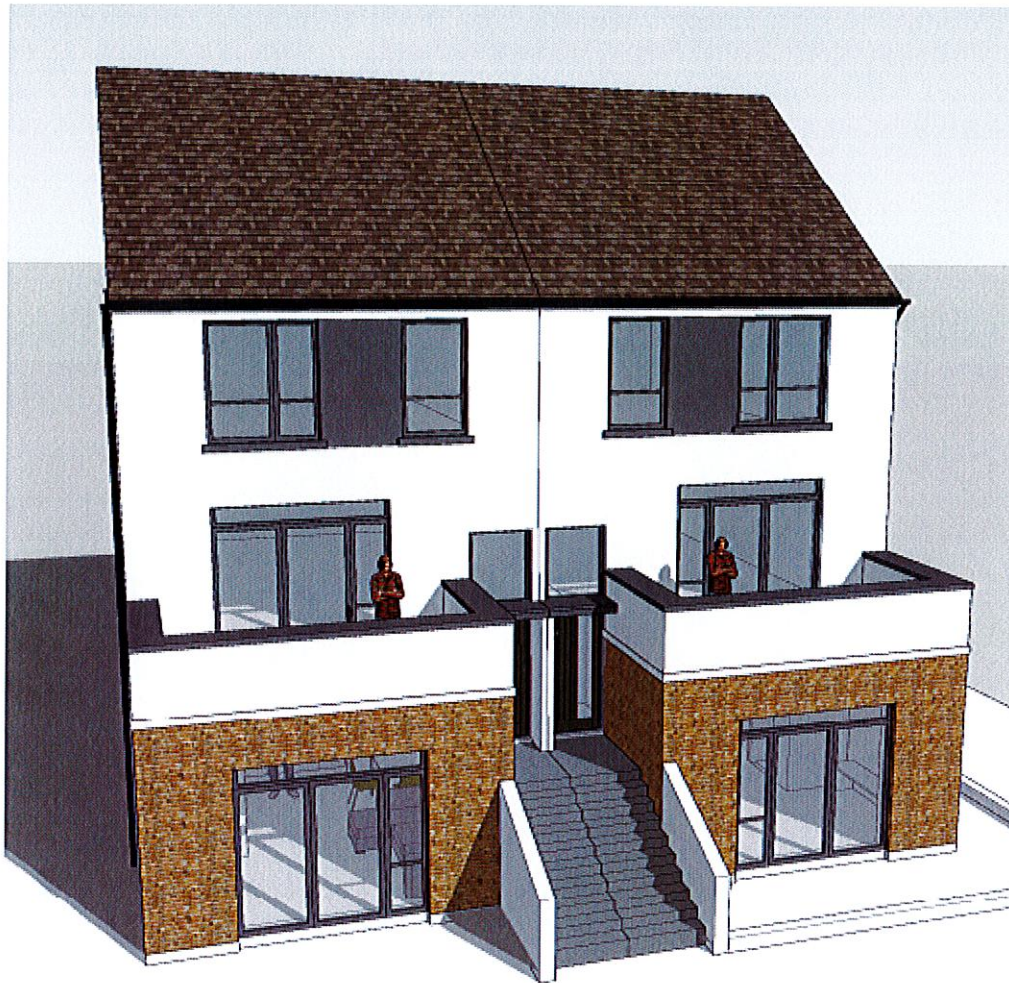
Brian

Attached is a plan of the proposed duplex that we are proposing as an option for Part V. It is a 2 and 3 bed mix. It can be revised in scale and layout to being a 1 and 2 bed. The image below is subject to design refinement. In principle, have you any objection to this being presented for Part V, subject to agreement as part of the proposal in Ballyvolane.



## HOUSE TYPE 5C - ELEVATIONS

3 Bed Duplex Town House  
2 Bed Ground Floor Apartment



FRONT ELE

HOUSE TYPE 4C& 4D

### John Crean

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---

**From:** Brian Geaney <brian\_geaney@corkcity.ie>  
**Sent:** 21 February 2019 17:57  
**To:** John Crean <John.Crean@temporis.ie>  
**Cc:** Alison O'Rourke <alison\_orourke@corkcity.ie>  
**Subject:** RE: Part V - Longview / Ballyvolane

John,

Duplex are of interest to the Council.

Regards,

Brian

---

**From:** John Crean [<mailto:John.Crean@temporis.ie>]  
**Sent:** 21 February 2019 17:39  
**To:** Brian Geaney  
**Cc:** Alison O'Rourke  
**Subject:** RE: Part V - Longview / Ballyvolane

Brian

Just following up on this question. Would an entire duplex be of interest to the Council?

## John Crean

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---

**From:** John Crean  
**Sent:** 19 February 2019 10:57  
**To:** 'Brian Geaney' <brian\_geaney@corkcity.ie>  
**Cc:** Alison O'Rourke <alison\_orourke@corkcity.ie>  
**Subject:** RE: Part V - Longview / Ballyvolane



Brian

I'd be obliged if you could have a look at this duplex unit and advise whether it is an option / format that would be of interest to the City. It's a model that has popped up on a site in Celbridge and we feel it may have merits although we are only reviewing the build options / costs / practicalities at present including M&E, how it might work with NZEB, servicing etc as it may be one of that suite of houses types out there that are practical only in the sense of offering more density but no practical viability.

If you consider the principle of this type of unit / mix as an option that you may be interested we may progress it further.

Much obliged.

## John Crean

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---

**From:** Brian Geaney <[brian\\_geaney@corkcity.ie](mailto:brian_geaney@corkcity.ie)>  
**Sent:** 15 February 2019 08:34  
**To:** John Crean <[John.Crean@temporis.ie](mailto:John.Crean@temporis.ie)>  
**Cc:** Alison O'Rourke <[alison\\_orourke@corkcity.ie](mailto:alison_orourke@corkcity.ie)>  
**Subject:** RE: Part V - Longview / Ballyvolane

John,

In principle, we do not have an objection to same.

Regards,

Brian

---

**From:** John Crean [<mailto:John.Crean@temporis.ie>]  
**Sent:** 13 February 2019 12:04  
**To:** Brian Geaney  
**Subject:** Part V - Longview / Ballyvolane

Brian

We may be looking at adding some duplex units to the proposal. Would the City Hall have an interest in Part V / Duplex units? Ground floor and upper floor options?

## John Crean

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## Orla O'Callaghan

---

**From:** Orla O'Callaghan  
**Sent:** 10 December 2019 12:30  
**To:** Orla O'Callaghan  
**Subject:** FW: Longview Estates - Irish Water

Hi John,  
Confirm I have no objection to a wayleave through Glen Valley Park on condition that it doesn't compromise or injure the amenities in the park. The implications, if any, will be considered following your scoping and survey of the route.

Regards,

Liam.

*Liam Casey  
Senior Exec Parks Supt  
Parks, Landscape & Cemeteries Division  
Cork City Council  
City Hall  
Cork*

*Tel...0214924275  
0862636346*



Comhairle Cathrach Chorcaí  
Cork City Council



**European Union**  
European Regional  
Development Fund



Tionól Réigiúnach an Deiscirt  
Southern Regional Assembly

---

**From:** John Crean [<mailto:John.Crean@temporis.ie>]  
**Sent:** 17 September 2019 12:28  
**To:** Liam Casey  
**Subject:** Longview Estates - Irish Water

Liam

Thanks for the meeting Friday 13<sup>th</sup>.

As discussed, and as presented at the SHD Meeting with ABP, City Council and Cork County Council, we are including the IW Pumping Station to service the UEA in the application. This will be served by a rising main that will be located in the public road. IW have statutory powers to locate the rising main in the public road; under the Water Services Act.

This rising main will go, from the pumping station, south along the Ballyhooley Road and onto the R635 Cork North Ring Road to allow onward connection to a point near the junction of the Old Youghal Road / R615 & R635 Cork North Ring Road.

An alternate route to the road may be to locate this rising main in the undeveloped Glen Valley park area to the east of the Ballyhooley Road (north of the R635) as IW already have foul mains in this area. We would like to take account of this potential route in our EIAR but obviously this would, if it were to be implemented require detailed design and appropriate easements (Wayleaves) over the route from Cork City Council Parks Dept.

Could you confirm that you would be agreeable to such easements (Wayleaves).

### **John Crean**

Head of Housing and Planning

Temporis Limited, Unit 74, Penrose Wharf, Penrose Quay, Cork, T23 HF51

Direct: +353 21 6017636 | Main: +353 21 6010286 | Mob: +353 86 8222896

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## **TEMPORIS**

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CUNNANE STRATTON REYNOLDS

Our ref: 17316/oooc/210519

Planning Notification  
Irish Water HQ Offices  
Coleville House  
24-26 Talbot Street  
Dublin 1

**By Post**

21<sup>st</sup> May 2019

Dear Sir/ Madam,

**Re: Strategic Housing Development Application. Longview Estates Ltd intend to apply to An Bord Pleanála for a 10 year planning permission for a strategic housing development of 753 no. residential units comprising of 59 detached, 276 semi-detached, 172 terrace, 92 duplexes, 154 apartments at Lahardane, Ballyvolane, Co. Cork. The proposed development includes the construction of a local centre (consisting of a crèche, doctors surgery, retail and community use), public parkland and open spaces and play areas, associated internal roads, pedestrian and cycle paths, boundary treatments, general landscaping, public lighting, internal bus stops and all site services infrastructure, two no. accesses from the Ballyhooley Road and all associated and ancillary site works. Signalisation of the Lower Dublin Hill / Ballyhooley Road Junction is also proposed along with the provision of a new bus stop on the western side of the Ballyhooley Road. Groundworks and excavation and local reprofiling are required and proposed to provide a Distributor Road and all development areas internally within the site. The proposed development also provides for the undergrounding of part of a 38 kv line (the Kilbarry-Flaxfort-Mayfield 38kv line) that traverses the landholding east / west, landscaping works within 110 kv wayleaves that also traverse the site; reservation of lands to accommodate the widening of the Ballyhooley Road and the provision of new pedestrian and cyclist infrastructure along the eastern side of the Ballyhooley Road with crossing of same and the provision of a new bus stop on Ballyhooley Road opposite the junction with Lower Dublin Hill and provision of a pumping station for Irish Water adjacent to the Ballyhooley Road, with access, to serve this site and future lands.**

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EMAIL: corkinfo@csrlandplan.ie

**OFFICES ALSO AT:**  
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EMAIL: info@csrlandplan.ie

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Yours Sincerely,



Orla O'Callaghan BA MPlan

**Senior Planner**

**CUNNANE STRATTON REYNOLDS**

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## CUNNANE STRATTON REYNOLDS

Our ref: 17316/oooc/210519

Minister Josepha Madigan  
23 Kildare Street  
Dublin 2  
D02 TD30

### By Post

21<sup>st</sup> May 2019

Dear Sir/ Madam,

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Yours Sincerely,



Orla O'Callaghan BA MPlan

**Senior Planner**

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CUNNANE STRATTON REYNOLDS

Our ref: 17316/oooc/210519

The Heritage Council  
Áras na hOidhreachta  
Church Lane  
Kilkenny  
P95 X264

**By Post**

21<sup>st</sup> May 2019

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CUNNANE STRATTON REYNOLDS

Our ref: 17316/oooc/210519

An Taisce The National Trust for Ireland  
Tailors' Hall  
Back Lane  
Dublin  
D08 X2A3

**By Post**

21<sup>st</sup> May 2019

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CUNNANE STRATTON REYNOLDS

Our ref: 17316/oooc/210519

Iarnród Éireann HQ,  
Connolly Station,  
Amien Street,  
Dublin 1,  
D01 V6V6

**By Post**

21<sup>st</sup> May 2019

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## CUNNANE STRATTON REYNOLDS

Our ref: 17316/oooc/210519

Transport Infrastructure Ireland  
Parkgate Business Centre  
Parkgate Street  
Dublin 8  
DO8 DK10

### By Post

21<sup>st</sup> May 2019

Dear Sir/ Madam,

**Re: Strategic Housing Development Application. Longview Estates Ltd intend to apply to An Bord Pleanála for a 10 year planning permission for a strategic housing development of 753 no. residential units comprising of 59 detached, 276 semi-detached, 172 terrace, 92 duplexes, 154 apartments at Lahardane, Ballyvolane, Co. Cork. The proposed development includes the construction of a local centre (consisting of a crèche, doctors surgery, retail and community use), public parkland and open spaces and play areas, associated internal roads, pedestrian and cycle paths, boundary treatments, general landscaping, public lighting, internal bus stops and all site services infrastructure, two no. accesses from the Ballyhooley Road and all associated and ancillary site works. Signalisation of the Lower Dublin Hill / Ballyhooley Road Junction is also proposed along with the provision of a new bus stop on the western side of the Ballyhooley Road. Groundworks and excavation and local reprofiling are required and proposed to provide a Distributor Road and all development areas internally within the site. The proposed development also provides for the undergrounding of part of a 38 kv line (the Kilbarry-Flaxfort-Mayfield 38kv line) that traverses the landholding east / west, landscaping works within 110 kv wayleaves that also traverse the site; reservation of lands to accommodate the widening of the Ballyhooley Road and the provision of new pedestrian and cyclist infrastructure along the eastern side of the Ballyhooley Road with crossing of same and the provision of a new bus stop on Ballyhooley Road opposite the junction with Lower Dublin Hill and provision of a pumping station for Irish Water adjacent to the Ballyhooley Road, with access, to serve this site and future lands.**

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Yours Sincerely,



Orla O'Callaghan BA MPlan

**Senior Planner**

**CUNNANE STRATTON REYNOLDS**

**LAND PLANNING & DESIGN**

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## CUNNANE STRATTON REYNOLDS

Our ref: 17316/ooc/210519

The National Transport Authority  
Dún Scéine  
Harcourt Street  
Dublin  
D02 WT20

### By Post

21<sup>st</sup> May 2019

Dear Sir/ Madam,

**Re: Strategic Housing Development Application. Longview Estates Ltd intend to apply to An Bord Pleanála for a 10 year planning permission for a strategic housing development of 753 no. residential units comprising of 59 detached, 276 semi-detached, 172 terrace, 92 duplexes, 154 apartments at Lahardane, Ballyvolane, Co. Cork. The proposed development includes the construction of a local centre (consisting of a crèche, doctors surgery, retail and community use), public parkland and open spaces and play areas, associated internal roads, pedestrian and cycle paths, boundary treatments, general landscaping, public lighting, internal bus stops and all site services infrastructure, two no. accesses from the Ballyhooley Road and all associated and ancillary site works. Signalisation of the Lower Dublin Hill / Ballyhooley Road Junction is also proposed along with the provision of a new bus stop on the western side of the Ballyhooley Road. Groundworks and excavation and local reprofiling are required and proposed to provide a Distributor Road and all development areas internally within the site. The proposed development also provides for the undergrounding of part of a 38 kv line (the Kilbarry-Flaxfort-Mayfield 38kv line) that traverses the landholding east / west, landscaping works within 110 kv wayleaves that also traverse the site; reservation of lands to accommodate the widening of the Ballyhooley Road and the provision of new pedestrian and cyclist infrastructure along the eastern side of the Ballyhooley Road with crossing of same and the provision of a new bus stop on Ballyhooley Road opposite the junction with Lower Dublin Hill and provision of a pumping station for Irish Water adjacent to the Ballyhooley Road, with access, to serve this site and future lands.**

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## CUNNANE STRATTON REYNOLDS

Our ref: 17316/oooc/210519

The Commission for Energy Regulation  
The Exchange  
Belgard Square North  
Tallaght  
Dublin 24

### By Post

21<sup>st</sup> May 2019

Dear Sir/ Madam,

**Re: Strategic Housing Development Application. Longview Estates Ltd intend to apply to An Bord Pleanála for a 10 year planning permission for a strategic housing development of 753 no. residential units comprising of 59 detached, 276 semi-detached, 172 terrace, 92 duplexes, 154 apartments at Lahardane, Ballyvolane, Co. Cork. The proposed development includes the construction of a local centre (consisting of a crèche, doctors surgery, retail and community use), public parkland and open spaces and play areas, associated internal roads, pedestrian and cycle paths, boundary treatments, general landscaping, public lighting, internal bus stops and all site services infrastructure, two no. accesses from the Ballyhooley Road and all associated and ancillary site works. Signalisation of the Lower Dublin Hill / Ballyhooley Road Junction is also proposed along with the provision of a new bus stop on the western side of the Ballyhooley Road. Groundworks and excavation and local reprofiling are required and proposed to provide a Distributor Road and all development areas internally within the site. The proposed development also provides for the undergrounding of part of a 38 kv line (the Kilbarry-Flaxfort-Mayfield 38kv line) that traverses the landholding east / west, landscaping works within 110 kv wayleaves that also traverse the site; reservation of lands to accommodate the widening of the Ballyhooley Road and the provision of new pedestrian and cyclist infrastructure along the eastern side of the Ballyhooley Road with crossing of same and the provision of a new bus stop on Ballyhooley Road opposite the junction with Lower Dublin Hill and provision of a pumping station for Irish Water adjacent to the Ballyhooley Road, with access, to serve this site and future lands.**

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## CUNNANE STRATTON REYNOLDS

Our ref: 17316/oooc/210519

Irish Aviation Authority  
The Times Building  
11-12 D'Olier Street  
Dublin 2

### By Post

21<sup>st</sup> May 2019

Dear Sir/ Madam,

**Re: Strategic Housing Development Application. Longview Estates Ltd intend to apply to An Bord Pleanála for a 10 year planning permission for a strategic housing development of 753 no. residential units comprising of 59 detached, 276 semi-detached, 172 terrace, 92 duplexes, 154 apartments at Lahardane, Ballyvolane, Co. Cork. The proposed development includes the construction of a local centre (consisting of a crèche, doctors surgery, retail and community use), public parkland and open spaces and play areas, associated internal roads, pedestrian and cycle paths, boundary treatments, general landscaping, public lighting, internal bus stops and all site services infrastructure, two no. accesses from the Ballyhooley Road and all associated and ancillary site works. Signalisation of the Lower Dublin Hill / Ballyhooley Road Junction is also proposed along with the provision of a new bus stop on the western side of the Ballyhooley Road. Groundworks and excavation and local reprofiling are required and proposed to provide a Distributor Road and all development areas internally within the site. The proposed development also provides for the undergrounding of part of a 38 kv line (the Kilbarry-Flaxfort-Mayfield 38kv line) that traverses the landholding east / west, landscaping works within 110 kv wayleaves that also traverse the site; reservation of lands to accommodate the widening of the Ballyhooley Road and the provision of new pedestrian and cyclist infrastructure along the eastern side of the Ballyhooley Road with crossing of same and the provision of a new bus stop on Ballyhooley Road opposite the junction with Lower Dublin Hill and provision of a pumping station for Irish Water adjacent to the Ballyhooley Road, with access, to serve this site and future lands.**

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## CUNNANE STRATTON REYNOLDS

Our ref: 17316/oooc/300719

Cork City Childcare Committee  
Suite 1, John Dunlop House  
Marina Commercial Park,  
Centre Park Road,  
Cork,

### By Email

30<sup>th</sup> July 2019

Dear Sir/ Madam,

**Re: Strategic Housing Development Application. Longview Estates Ltd intend to apply to An Bord Pleanála for a 10 year planning permission for a strategic housing development of 753 no. residential units comprising of 59 detached, 276 semi-detached, 172 terrace, 92 duplexes, 154 apartments at Lahardane, Ballyvolane, Co. Cork. The proposed development includes the construction of a local centre (consisting of a crèche, doctors surgery, retail and community use), public parkland and open spaces and play areas, associated internal roads, pedestrian and cycle paths, boundary treatments, general landscaping, public lighting, internal bus stops and all site services infrastructure, two no. accesses from the Ballyhooley Road and all associated and ancillary site works. Signalisation of the Lower Dublin Hill / Ballyhooley Road Junction is also proposed along with the provision of a new bus stop on the western side of the Ballyhooley Road. Groundworks and excavation and local reprofiling are required and proposed to provide a Distributor Road and all development areas internally within the site. The proposed development also provides for the undergrounding of part of a 38 kv line (the Kilbarry-Flaxfort-Mayfield 38kv line) that traverses the landholding east / west, landscaping works within 110 kv wayleaves that also traverse the site; reservation of lands to accommodate the widening of the Ballyhooley Road and the provision of new pedestrian and cyclist infrastructure along the eastern side of the Ballyhooley Road with crossing of same and the provision of a new bus stop on Ballyhooley Road opposite the junction with Lower Dublin Hill and provision of a pumping station for Irish Water adjacent to the Ballyhooley Road, with access, to serve this site and future lands.**

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## Appendix D Part V Calculations



Longview  
House Type Part V - 2 Bed Mid Terrace  
Part V Cost & Methodology - House Cost Summaries

**House Cost Summaries**

Unit Size	87.64
No. of Units	17
Total No. of Units on-site	753
Site Area	23.5
<b>Assumed Costs</b>	
Construction Cost (per sq.m)	€1,400
Site Work Cost (per sq.m)	€371
Existing Use Value (per ha)	€36,000
<hr/>	
<b>Construction Costs (excluding construction profit)</b>	
Estimated Construction Cost per unit [1]@ €1,453 per sq.m	€122,696
Estimated Site Works & Servicing Cost per unit [2]	€32,514
<b>Sub Total</b>	<b>€155,210</b>
<hr/>	
Attributable Development Costs @ 22.50% of Construction Cost [3]	€34,922
Design Team Fees	
Legal & Estate Agent Fees	
Marketing Costs	
<b>Sub-total ex- VAT</b>	<b>€190,133</b>
<hr/>	
Profit on Construction Costs (but not attributable development costs)@15%	€23,282
<hr/>	
House & Land Cost -(ex VAT)	€213,414
VAT @ 13.5%	€28,811
Total Cost (inc. VAT) but <b>excluding</b> Development Contributions / LA Bonds[5]	€242,225
<hr/>	
Site Cost per unit (existing use value - EUV [4]) Site Area/Units	€1,124
Vat on Land	€152
<b>Overall House Cost, with VAT and EUV of land</b>	<b>€243,500</b>
<hr/> <hr/>	

Longview  
House Type Part V - 3 Bed End of Terrace  
Part V Cost & Methodology - House Cost Summaries

**House Cost Summaries**

Unit Size	107.02
No. of Units	11
Total No. of Units on-site	753
Site Area	23.5

**Assumed Costs**

Construction Cost (per sq.m)	€1,400
Site Work Cost (per sq.m)	€371
Existing Use Value (per ha)	€36,000

**Construction Costs** (excluding construction profit)

Estimated Construction Cost per unit [1]@ €1,453 per sq.m	€149,828
Estimated Site Works & Servicing Cost per unit [2]	€39,704
<b>Sub Total</b>	<b>€189,532</b>

Attributable Development Costs @ 22.5% of Construction Cost [3]	€42,645
Design Team Fees	
Legal & Estate Agent Fees	
Marketing Costs	
<b>Sub-total ex- VAT</b>	<b>€232,177</b>

Profit on Construction Costs (but not attributable development costs)@15%	€28,430
---	---------

House & Land Cost -(ex VAT)	€260,607
VAT @ 13.5%	€35,182
<b>Total Cost (inc. VAT) but <b>excluding</b> Development Contributions / LA Bonds[5]</b>	<b>€295,789</b>

Site Cost per unit (existing use value - EUV [4]) Site Area/Units	€1,124
Vat on Land	€152
<b>Overall House Cost, with VAT and EUV of land</b>	<b>€297,064</b>



Longview  
House Type Part V - 3 Bed Mid Terrace  
Part V Cost & Methodology - House Cost Summaries

**House Cost Summaries**

Unit Size	107.02
No. of Units	8
Total No. of Units on-site	753
Site Area	23.5

**Assumed Costs**

Construction Cost (per sq.m)	€1,400
Site Work Cost (per sq.m)	€371
Existing Use Value (per ha)	€36,000

**Construction Costs** (excluding construction profit)

Estimated Construction Cost per unit [1]@ €1,453 per sq.m	€149,828
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Site Cost per unit (existing use value - EUV [4]) Site Area/Units	€1,124
Vat on Land	€152
<b>Overall House Cost, with VAT and EUV of land</b>	<b>€297,064</b>

Longview  
House Type Part V - 3 Bed Semi Detached  
Part V Cost & Methodology - House Cost Summaries

**House Cost Summaries**

Unit Size	110.4
No. of Units	2
Total No. of Units on-site	753
Site Area	23.5

**Assumed Costs**

Construction Cost (per sq.m)	€1,400
Site Work Cost (per sq.m)	€401
Existing Use Value (per ha)	€36,000

**Construction Costs** (excluding construction profit)

Estimated Construction Cost per unit [1]@ €1,453 per sq.m	€154,560
Estimated Site Works & Servicing Cost per unit [2]	€44,270
<b>Sub Total</b>	<b>€198,830</b>

Attributable Development Costs @ 22.50% of Construction Cost [3]	€44,737
Design Team Fees	
Legal & Estate Agent Fees	
Marketing Costs	
<b>Sub-total ex- VAT</b>	<b>€243,567</b>

Profit on Construction Costs (but not attributable development costs)@15%	€29,825
---	---------

House & Land Cost -(ex VAT)	€273,392
VAT @ 13.5%	€36,908
<b>Total Cost (inc. VAT) but <b>excluding</b> Development Contributions / LA Bonds[5]</b>	<b>€310,300</b>

Site Cost per unit (existing use value - EUV [4]) Site Area/Units	€1,124
Vat on Land	€152
<b>Overall House Cost, with VAT and EUV of land</b>	<b>€311,575</b>



Longview  
House Type Part V - 3 Bed Semi Detached  
Part V Cost & Methodology - House Cost Summaries

**House Cost Summaries**

Unit Size	110.4
No. of Units	2
Total No. of Units on-site	753
Site Area	23.5

**Assumed Costs**

Construction Cost (per sq.m)	€1,400
Site Work Cost (per sq.m)	€401
Existing Use Value (per ha)	€36,000

**Construction Costs** (excluding construction profit)

Estimated Construction Cost per unit [1]@ €1,453 per sq.m	€154,560
Estimated Site Works & Servicing Cost per unit [2]	€44,270
<b>Sub Total</b>	<b>€198,830</b>

Attributable Development Costs @ 22.50% of Construction Cost [3]	€44,737
Design Team Fees	
Legal & Estate Agent Fees	
Marketing Costs	
<b>Sub-total ex- VAT</b>	<b>€243,567</b>

Profit on Construction Costs (but not attributable development costs)@15%	€29,825
---	---------

House & Land Cost -(ex VAT)	€273,392
VAT @ 13.5%	€36,908
<b>Total Cost (inc. VAT) but <b>excluding</b> Development Contributions / LA Bonds[5]</b>	<b>€310,300</b>

Site Cost per unit (existing use value - EUV [4]) Site Area/Units	€1,124
Vat on Land	€152
<b>Overall House Cost, with VAT and EUV of land</b>	<b>€311,575</b>

Longview  
House Type Part V - 4 Bed Semi Detached  
Part V Cost & Methodology - House Cost Summaries

**House Cost Summaries**

Unit Size	114.24
No. of Units	1
Total No. of Units on-site	753
Site Area	23.5

**Assumed Costs**

Construction Cost (per sq.m)	€1,400
Site Work Cost (per sq.m)	€411
Existing Use Value (per ha)	€36,000

**Construction Costs** (excluding construction profit)

Estimated Construction Cost per unit [1]@ €1,453 per sq.m	€159,936
Estimated Site Works & Servicing Cost per unit [2]	€46,953
<b>Sub Total</b>	<b>€206,889</b>

Attributable Development Costs @ 22.50% of Construction Cost [3]	€46,550
Design Team Fees	
Legal & Estate Agent Fees	
Marketing Costs	
<b>Sub-total ex- VAT</b>	<b>€253,439</b>

Profit on Construction Costs (but not attributable development costs)@15%	€31,033
---	---------

House & Land Cost -(ex VAT)	€284,472
VAT @ 13.5%	€38,404
<b>Total Cost (inc. VAT) but excluding Development Contributions / LA Bonds[5]</b>	<b>€322,876</b>

Site Cost per unit (existing use value - EUV [4]) Site Area/Units	€1,124
Vat on Land	€152
<b>Overall House Cost, with VAT and EUV of land</b>	<b>€324,151</b>



Longview  
House Type Part V - 2 Bed Bottom Duplex End  
Part V Cost & Methodology - House Cost Summaries

**House Cost Summaries**

Unit Size	63.63
No. of Units	7
Total No. of Units on-site	753
Site Area	23.5

**Assumed Costs**

Construction Cost (per sq.m)	€1,714
Site Work Cost (per sq.m)	€321
Existing Use Value (per ha)	€36,000

**Construction Costs** (excluding construction profit)

Estimated Construction Cost per unit [1]@ €1,453 per sq.m	€109,062
Estimated Site Works & Servicing Cost per unit [2]	€20,425
<b>Sub Total</b>	<b>€129,487</b>

Attributable Development Costs @ 20% of Construction Cost [3]	€25,897
Design Team Fees	
Legal & Estate Agent Fees	
Marketing Costs	
<b>Sub-total ex- VAT</b>	<b>€155,384</b>

Profit on Construction Costs (but not attributable development costs)@15%	€19,423
---	---------

House & Land Cost -(ex VAT)	€174,808
VAT @ 13.5%	€23,599
<b>Total Cost (inc. VAT) but <b>excluding</b> Development Contributions / LA Bonds[5]</b>	<b>€198,407</b>

Site Cost per unit (existing use value - EUV [4]) Site Area/Units	€1,124
Vat on Land	€152
<b>Overall House Cost, with VAT and EUV of land</b>	<b>€199,682</b>

Longview  
House Type Part V - 2 Bed Lower Duplex Mid  
Part V Cost & Methodology - House Cost Summaries

**House Cost Summaries**

Unit Size	63.63
No. of Units	10
Total No. of Units on-site	753
Site Area	23.5

**Assumed Costs**

Construction Cost (per sq.m)	€1,711
Site Work Cost (per sq.m)	€321
Existing Use Value (per ha)	€36,000

**Construction Costs** (excluding construction profit)

Estimated Construction Cost per unit [1]@ €1,453 per sq.m	€108,871
Estimated Site Works & Servicing Cost per unit [2]	€20,425
<b>Sub Total</b>	<b>€129,296</b>

Attributable Development Costs @ 20% of Construction Cost [3]	€25,859
Design Team Fees	
Legal & Estate Agent Fees	
Marketing Costs	
<b>Sub-total ex- VAT</b>	<b>€155,155</b>

Profit on Construction Costs (but not attributable development costs)@15%	€19,394
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House & Land Cost -(ex VAT)	€174,550
VAT @ 13.5%	€23,564
Total Cost (inc. VAT) but <b>excluding</b> Development Contributions / LA Bonds[5]	€198,114

Site Cost per unit (existing use value - EUV [4]) Site Area/Units	€1,124
Vat on Land	€152
<b>Overall House Cost, with VAT and EUV of land</b>	<b>€199,389</b>



Longview  
House Type Part V - 3 Bed Upper Duplex End  
Part V Cost & Methodology - House Cost Summaries

**House Cost Summaries**

Unit Size	108.88
No. of Units	9
Total No. of Units on-site	753
Site Area	23.5

**Assumed Costs**

Construction Cost (per sq.m)	€1,520
Site Work Cost (per sq.m)	€321
Existing Use Value (per ha)	€36,000

**Construction Costs** (excluding construction profit)

Estimated Construction Cost per unit [1]@ €1,453 per sq.m	€165,498
Estimated Site Works & Servicing Cost per unit [2]	€34,950
<b>Sub Total</b>	<b>€200,448</b>

Attributable Development Costs @ 20% of Construction Cost [3]	€40,090
Design Team Fees	
Legal & Estate Agent Fees	
Marketing Costs	
<b>Sub-total ex- VAT</b>	<b>€240,538</b>

Profit on Construction Costs (but not attributable development costs)@15%	€30,067
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House Cost	€270,605
VAT @ 13.5%	€36,532
<b>Total Cost (inc. VAT) but <b>excluding</b> Development Contributions / LA Bonds[5]</b>	<b>€307,137</b>

Site Cost per unit (existing use value - EUV [4]) Site Area/Units	€1,124
Vat on Land	€152
<b>Overall House Cost, with VAT and EUV of land</b>	<b>€308,412</b>

Longview  
House Type Part V - 3 Bed Upper Duplex Mid  
Part V Cost & Methodology - House Cost Summaries

**House Cost Summaries**

Unit Size	108.88
No. of Units	10
Total No. of Units on-site	753
Site Area	23.5

**Assumed Costs**

Construction Cost (per sq.m)	€1,510
Site Work Cost (per sq.m)	€321
Existing Use Value (per ha)	€36,000

**Construction Costs** (excluding construction profit)

Estimated Construction Cost per unit [1]@ €1,453 per sq.m	€164,409
Estimated Site Works & Servicing Cost per unit [2]	€34,950
<b>Sub Total</b>	<b>€199,359</b>

Attributable Development Costs @ 20% of Construction Cost [3]	€39,872
Design Team Fees	
Legal & Estate Agent Fees	
Marketing Costs	
<b>Sub-total ex- VAT</b>	<b>€239,231</b>

Profit on Construction Costs (but not attributable development costs)@15%	€29,904
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House Cost	€269,135
VAT @ 13.5%	€36,333
<b>Total Cost (inc. VAT) but <b>excluding</b> Development Contributions / LA Bonds[5]</b>	<b>€305,468</b>

Site Cost per unit (existing use value - EUV [4]) Site Area/Units	€1,124
Vat on Land	€152
<b>Overall House Cost, with VAT and EUV of land</b>	<b>€306,743</b>